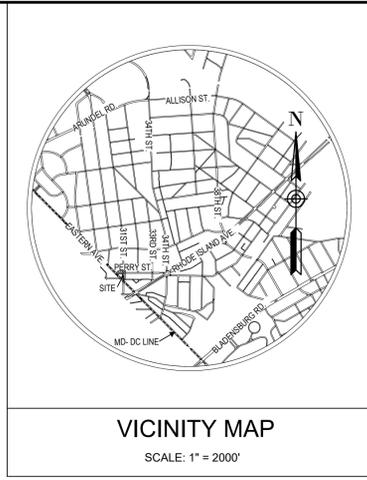


SHEET INDEX

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GENERAL STORM DRAIN AND PAVING NOTES

- 1) INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATION OF THE MAINS BY DIGGING TEST PITS, BY HAND OR VACUUM, AT UTILITY CROSSINGS WELL IN ADVANCE OF TRENCHING. IF CLEARANCES TO WATER AND SEWER LINES ARE LESS THAN SHOWN ON THIS PLAN, OR LESS THAN TWELVE (12) INCHES, CONTACT THE DEPARTMENT OF PERMITTING INSPECTION AND ENFORCEMENT (DPIE) INSPECTOR BEFORE PROCEEDING WITH CONSTRUCTION.
- 2) ALL STORM DRAINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITION OF THE STORMWATER MANAGEMENT STANDARDS AND SPECIFICATIONS OF PRINCE GEORGE'S COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION.
- 3) FOR TYPES OF STORM DRAIN STRUCTURES, REFER TO THE LATEST STANDARD DETAILS OF PRINCE GEORGE'S COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION, UNLESS OTHERWISE NOTED.
- 4) ALL ROADWAY CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE FOLLOWING: THE DPW&T SPECIFICATIONS AND STANDARDS FOR ROADWAYS AND BRIDGES: THE PRINCE GEORGE'S COUNTY CODE, SUBTITLE 23, ROAD ORDINANCE; AND THE PRINCE GEORGE'S COUNTY POLICY AND SPECIFICATION FOR UTILITY INSTALLATION AND MAINTENANCE.
- 5) PRIOR TO DIGGING WITHIN THE PUBLIC RIGHT-OF-WAY, CALL "MISS UTILITY" TOLL FREE AT (800) 257-7777 FOR UTILITY LOCATION AT LEAST 48 HOURS BEFORE BEGINNING CONSTRUCTION.
- 6) PRIOR TO PERMIT ISSUANCE AND STARTING ANY WORK SHOWN ON THIS PLAN, THE PERMITTEE SHALL ARRANGE A PRE-CONSTRUCTION MEETING WITH THE DPIE INSPECTOR BY CALLING (301) 883-5730. AN INITIAL INSPECTION IS REQUIRED PRIOR TO FULL MASS GRADING OF THE SITE.
- 7) IN ACCORDANCE WITH SECTION 23-128, THE COUNTY'S ROAD ORDINANCE, A PROJECT SIGN SHALL BE POSTED PROMINENTLY DESCRIBING THE FOLLOWING: SUBDIVISION NAME (AS SHOWN ON PERMIT APPLICATION) -OWNER/PERMITTEE NAME-OWNER/PERMITTEE ADDRESS AND PHONE-DPW&T PERMIT NUMBER
- 8) ALL ELEVATIONS SHOWN ON THIS PLAN ARE IN ACCORDANCE WITH THE FOLLOWING: HORIZONTAL—MARYLAND COORDINATE SYSTEM (STATE PLANE GRID) BASED ON NORTH AMERICAN DATUM OF 1983 (NAD 83); NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
- 9) TEMPORARY TRAFFIC CONTROL AND PERMANENT TRAFFIC SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE FEDERAL HIGHWAY ADMINISTRATION'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- 10) IT SHALL BE THE RESPONSIBILITY OF THE PERMITTEE TO ARRANGE FOR THE ADJUSTMENT OR RELOCATION OF ALL UTILITIES.
- 11) ALL UNSUITABLE MATERIAL MUST BE REMOVED AND REPLACED WITH SUITABLE MATERIAL TO A DEPTH AS DIRECTED BY THE GEOTECHNICAL ENGINEER, THE DPW&T INSPECTOR, AND/OR THE DEPARTMENT'S ENGINEER.
- 12) EXCAVATION AND PLACEMENT OF FILL MATERIAL SHALL BE PERFORMED UNDER THE SUPERVISION OF A MARYLAND-LICENSED ENGINEER.
- 13) THE PERMITTEE WILL BE REQUIRED TO FURNISH COMPACTION REPORTS CERTIFIED BY A MARYLAND-LICENSED ENGINEER ON EACH LAYER OF FILL MATERIAL PRIOR TO PLACING SUBSEQUENT LAYERS.
- 14) DURING THE PLACEMENT OF A STANDARD PAVEMENT SECTION, NO PAVEMENT COURSE OR STONE LIFT SHALL BE PLACED UNTIL THE UNDERLYING COURSE OR SUBGRADE IS APPROVED BY THE DPW&T INSPECTOR. THE APPROVAL SHALL EXPIRE IF TRAFFIC OR INCLEMENT WEATHER AFFECTS THE SITE PRIOR TO PAVING.
- 15) AS SOON AS THE ASPHALT BASE COURSE IS APPROVED, THE INTERMEDIATE ASPHALT COURSE SHALL BE PLACED IMMEDIATELY OVER IT TO FORM A PROTECTIVE SEAL.
- 16) TEMPORARY STREET NAME SIGN INSTALLATION AND MAINTENANCE IS THE OBLIGATION OF THE PERMITTEE ONCE BASE PAVING IS COMPLETED.
- 17) WHERE ROADWAY CONSTRUCTION IS ON OR IN THE VICINITY OF AN EXISTING ROAD, IN-KIND PAVEMENT MARKING AND STRIPING REPLACEMENT (E.G., THERMOPLASTIC, PAINTED, ETC.) ARE REQUIRED. ALSO, APPROPRIATE PAVEMENT MARKING AND STRIPING SHALL BE PROVIDED IN THE AREA OF PAVEMENT WIDENING AND/OR RECONSTRUCTION AND/OR OVERLAY OF AN EXISTING ROAD.
- 18) SAW CUT AND MILL A 2-INCH DEEP, 10-FOOT-WIDE NOTCH AT EXISTING EDGE OF PAVEMENT WHERE IT IS NECESSARY TO CONNECT TO OR TO EXTEND AN EXISTING ROAD, OVERLAY AT POINT OF TIE-IN TO ENSURE A SMOOTH TRANSITION AND POSITIVE DRAINAGE.
- 19) WHERE IT IS NECESSARY TO WIDEN AN EXISTING ROAD, AND MILLING AND

- OVERLAY REQUIREMENTS HAVE BEEN WAIVED OR REDUCED, THE WIDENING AND THE EDGE TREATMENT OF EXISTING ROAD SHALL BE CONSTRUCTED IN ACCORDANCE WITH DPW&T STANDARD NO.300.20 UNLESS OTHERWISE DIRECTED BY THE DEPARTMENT.
- 20) ALL RESIDENTIAL ROADWAY FILLET RADII SHALL BE AT LEAST 37 FEET, UNLESS OTHERWISE NOTED. ROADWAYS WITH HIGHER CLASSIFICATION REQUIRE 45 FEET AND/OR 50 FEET RADII.
- 21) AN UNDERDRAIN SYSTEM IS REQUIRED FOR THE FULL LENGTH OF ALL PROPOSED AND MODIFIED ROADWAYS, ON BOTH SIDES, AND TO THE LIMITS OF THE PERMIT SHOWN ON THIS PLAN.
- 22) ALL CURB AND GUTTER SHALL BE CONSTRUCTED IN ACCORDANCE WITH DPW&T STANDARDS NO.300.01 THROUGH 300.04 UNLESS OTHERWISE DIRECTED BY THE DEPARTMENT.
- 23) BRICK CHANNELIZATION IS REQUIRED IN ALL PUBLIC DPW&T STORM DRAIN STRUCTURES. CONCRETE CHANNELIZATION IS NOT ALLOWED.
- 24) POSITIVE DRAINAGE SHALL BE MAINTAINED THROUGHOUT THE AREA COVERED BY THIS PERMIT AND THROUGH ADJACENT PROPERTY FRONTAGES.
- 25) ALL UNPAVED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE SODDED.
- 26) ALL SIDEWALK RAMP SHOWN ON THIS PLAN SHALL BE CONSTRUCTED IN ACCORDANCE WITH DPW&T STANDARDS 300.05 THROUGH 300.10 AND SHALL COMPLY WITH THE LATEST REVISION TO THE FEDERAL ACCESSIBILITY GUIDELINES OF THE AMERICANS WITH DISABILITIES ACT.
- 27) ALL SIDEWALKS SHOWN ON THIS PLAN SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST DPW&T STANDARDS AND SHALL COMPLY WITH THE LATEST REVISION TO THE FEDERAL ACCESSIBILITY GUIDELINES OF AMERICANS WITH DISABILITIES ACT.
- 28) ALL SIDEWALKS (EXCEPT AS NOTED HEREIN) ARE TO BE CONSTRUCTED BY THE SITE DEVELOPER.
- 29) SIDEWALKS ALONG FRONTAGES OF OPEN-SPACE PARCELS AND THOSE NOT COVERED BY A SINGLE-FAMILY BUILDING PERMIT SHALL BE CONSTRUCTED UNDER THIS STREET CONSTRUCTION PERMIT.
- 30) THE WIDTH OF A RESIDENTIAL DRIVEWAY APRON AT THE PROPERTY LINE SHALL NOT BE LESS THAN THE WIDTH OF THE ONSITE PARKING PAD AT ITS WIDEST POINT, A MAXIMUM WIDTH OF 20 FEET, AND A MINIMUM WIDTH OF 10 FEET. A RESIDENTIAL DRIVEWAY APRON FLARE SHALL NOT BE CONSTRUCTED CLOSER THAN 3.5 FEET TO THE NEAREST ABUTTING PROPERTY LINE.
- 31) ALL DRIVEWAY APRONS ARE TO BE CONSTRUCTED BY THE SITE DEVELOPER.
- 32) ENSURE THAT STREET TREES ARE NO CLOSER THAN 1 FOOT TO THE RIGHT-OF-WAY LINE, IN AN OPEN SPACE SECTION CONFIGURATION, AND NO CLOSER THAN 15 FEET FROM STREET LIGHT OR POLE, AND OF APPROPRIATE HEIGHT SO AS NOT TO INTERFERE WITH EXISTING OR PROPOSED OVERHEAD UTILITY LINES. ALL STREETSCAPE PLANTING SHALL BE IN ACCORDANCE WITH DPW&T STANDARDS 600.01 THROUGH 600.20 UNLESS DIRECTED OTHERWISE BY THE DEPARTMENT.
- 33) PAVEMENT QUALITY CONTROL AND CORING WILL BE REQUIRED OF THE PERMITTEE FOR ALL PAVEMENT COURSES USING THE FOLLOWING PROCESS:
 - a. PRIOR TO PAVING, PERMITEE/PAVING CONTRACTOR SHALL ELECTRONICALLY NOTIFY INSPECTOR OF PAVING DATES (FAX CAN OCCASIONALLY BE ACCEPTED), AND PROVIDE INFORMATION NEEDED FOR CORE TESTING REQUEST (CTR FORM #1). DPW&T INSPECTOR FILLS OUT THE CTR FORM THEN SENDS IT TO THE MATERIALS LAB.
 - b. CONTRACTORS, WHO ARE NEW TO THE MATERIALS LAB, SHALL ELECTRONICALLY SUBMIT QC PLANS TO IT, AND ARRANGE TO BE INITIATED BY IT PRIOR TO PAVING.
 - c. THE PERMITEE/PAVING CONTRACTOR MUST PROVIDE A MD-CERTIFIED FIELD TECHNICIAN FOR DAILY QUALITY CONTROL (QC) TESTING DURING THE ENTIRE PAVING OPERATION (NOT JUST ITS END). FIELD TECHNICIAN SHALL BE ADEQUATELY EQUIPPED WITH A PHONE, CALIBRATED THERMOMETER, AND A CALIBRATED THIN-LIFT DENSITY GAUGE FOR QC AND INSPECTOR-REQUESTED TESTING.
 - d. HMA DENSITY GAUGES SHALL BE VALIDATED AND CALIBRATED DAILY (SHA 504.03.10. A.2), SO THEIR READINGS CAN BE ACCEPTED FOR COURSE PLACED WITH A TOTAL TONNAGE UNDER 200 TONS OR ACCEPTABLE THICKNESS UNDER 1.2' DUE TO PATCHING, WEDGE & LEVELING, BRIDGE DECKS, ... ETC.
 - e. FIELD TECH. SHALL CALL THE MATERIALS LAB WITH DATE & TIME OF CORE CUTTING SO IT CAN BE WITNESSED, AND CORES & HMA SAMPLES CAN BE RECEIVED ON SITE BY A LAB INSPECTOR.
 - f. INSPECTOR RANDOMLY SELECTS & MARKS CORE LOCATIONS IN THE FIELD; NOTES THEM DOWN ON THE CTR STATING STREET'S NAME AND ADDRESS, LOT #, STA #, OR DISTANCE FROM NEAREST INTERSECTION; THEN FAXES THE CTR AGAIN TO THE LAB, AND GIVES TO FIELD TECH BOXES FOR ONLY PR. GEORGE'S COUNTY HMA SAMPLES.
 - g. FIELD TECH. SHALL CHECK DELIVERY TICKETS FOR COUNTY-REQUIRED INFO, COLLECT AT LEAST ONE BEHIND-THE-PAVER HMA SAMPLE/MIX/DAY, AND CUT AT LEAST 5 CORES/MIX/DAY BUT NO LESS THAN 2 CORES FROM EACH

- STREET, UNLESS OTHERWISE INSTRUCTED BY AN INSPECTOR; THEN HAND OVER THE SAMPLES TO DPW&T INSPECTOR NO LATER THAN ONE (1) BUSINESS DAY FROM THE PAVING.
- h. IF, AT THE LAB'S DISCRETION, THE CORES' CUTTING IS NOT WITNESSED, CORES SHALL BE RECEIVED IN THE LAB, IN ONE BUSINESS DAY FROM PAVING, AS LONG AS THEY ARE NUMBERED AND WELL IDENTIFIED ON FORM #2 BY: PROJECT NAME, STREET NAME, CORE LOCATION, PAVING DATE, CORING DATE, MIX CODE, ... ETC. IF NOT PROPERLY IDENTIFIED, CORES WILL NOT BE ACCEPTED.
- i. IF, AT THE LAB'S DISCRETION, A BEHIND-THE-PAVER HMA SAMPLE IS NOT RECEIVED ON SITE, IT SHALL BE RECEIVED IN THE LAB ALONG WITH CERTIFIED DELIVERY TICKETS, IN ONE (1) BUSINESS DAY, AND BE IDENTIFIED BY: PROJECT NAME, SAMPLING LOCATION, PAVING DATE, & STATE MIX DESIGNATION. IF NOT PROPERLY LABELED, HMA SAMPLES WILL NOT BE ACCEPTED.
- j. IF CORES ARE TESTED AT AN INDEPENDENT THIRD PARTY'S TESTING LAB, THAT LAB MUST BE AASHTO ACCREDITED FOR SPECIFIC TESTS, AND BE INITIATED BY THE MATERIALS LAB, WHICH SHALL BE NOTIFIED (FAX CAN OCCASIONALLY BE ACCEPTED), OF THE TESTING DATE & TIME SO IT MAY WITNESS THE 3RD PARTY TESTING.
- k. CORE RESULTS SHALL BE REPORTED ON CORE ANALYSIS (FORM #2), E-MAILED TO MATERIALS LAB & INSPECTOR NO LATER THAN ONE BUSINESS DAY FROM TESTING (ONE DAY FROM CUTTING FOR COMPANION CORES), AND MAILED OUT TO PERMITEE.
- l. FIELD TECHNICIANS AND THIRD PARTY TESTING LABS SHALL MAINTAIN A LOG OF THEIR TEST RESULTS; RECOMMENDATIONS, AND ACTIONS TAKEN TO CORRECT THE PROBLEMS, IF ANY. THE LOG SHALL BE AVAILABLE TO DPW&T FOR REVIEW UPON ANY DPW&T INSPECTOR'S REQUEST.
- 34) PERMITEE SHALL SUBMIT PROPERTY CORNER CERTIFICATIONS AND UTILIZE METAL PROPERTY MARKERS PER PRINCE GEORGE'S COUNTY CODE, SECTION 24-120, PRIOR TO ACCEPTANCE OF STREETS.
- 35) WASHINGTON SUBURBAN SANITARY COMMISSION
200 FOOT SHEET NO.: 205NE03
- 36) DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION SITE CONCEPT APPROVAL NUMBER: N/A
- 37) SEDIMENT CONTROL APPROVAL NUMBER: N/A
- 38) PRELIMINARY PLAN APPROVAL NUMBER: N/A
- 39) RECORD PLAT RECORDING NUMBER: N/A
- 40) APPROVED STREET GRADE ESTABLISHMENT INFORMATION: N/A
- 41) AT THE TIME OF PERMIT RELEASE, THE FOLLOWING MINIMUM SUBMITTAL REQUIREMENTS WHERE APPLICABLE SHALL APPLY:
 - WASHINGTON SUBURBAN SANITARY COMMISSION PAVING CLEARANCE CERTIFICATION;
 - BITUMINOUS CONCRETE CORE CERTIFICATIONS, ALL PAVEMENT COURSES;
 - PROPERTY MARKER CERTIFICATION;
 - DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION LETTER APPROVING STORM DRAIN AS-BUILT;
 - TREE APPROVAL AND TREE BOND POSTED, IF NECESSARY;
 - STREET LIGHTPROOF OF PAYMENT (MUST BE ACCOMPANIED BY A MEMORANDUM FROM DPW&T'S TRAFFIC SAFETY DIVISION ACCEPTING THE PROOF OF PAYMENT); AND
 - PROOF/STATEMENT THAT ALL FINANCIAL MATTERS HAVE BEEN SETTLED.
- 42) THE PERMITTEE IS RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF ALL TRAFFIC SIGNS, TRAFFIC SIGNALS, AND ROADWAY MARKINGS FOR ROADWAY IMPROVEMENTS ON SUBDIVISION ACCESS ROADS WHICH INCLUDE ARTERIAL, COLLECTOR, INDUSTRIAL, AND ANY NECESSARY OFFSITE CONDITIONS WHICH REQUIRE ROADWAY IMPROVEMENTS. THE DESIGN AND/OR CONSTRUCTION DRAWINGS SHALL BE INCLUDED ALONG WITH THE PERMIT PLANS; AND SHALL BE REVIEWED AND APPROVED BY THE DEPARTMENT'S TRAFFIC SAFETY DIVISION PRIOR TO PERMIT ISSUANCE.
- 43) THE PERMITTEE IS RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF ALL TRAFFIC MARKINGS, TRAFFIC SIGNALS, IF REQUIRED, AND PAYMENT OF FEE FOR STREET NAME SIGNS ON INTERNAL SUBDIVISION STREETS. TRAFFIC SIGNS WILL BE FURNISHED AND INSTALLED BY THE COUNTY.
- 44) ALL CONCRETE PIPE SYSTEMS 48 INCHES OR LESS SHALL BE INSPECTED WITH A VIDEO CAMERA AS PART OF THE FINAL "AS BUILT" CONSTRUCTION REQUIREMENT.
- 45) ALL STORM DRAIN PIPES MUST HAVE A MINIMUM OF 1 FOOT COVER.
- 46) ALL INLET TOP SLAB FRONT FACES SHALL BE PAINTED WITH THE FOLLOWING CHESAPEAKE BAY DRAINAGE, "DON'T DUMP" (STANDARD 82.0).
- 47) CONTRACTORS SHALL ADJUST ALL EXISTING UTILITIES AS NEEDED TO CONSTRUCT PROPOSED ROAD IMPROVEMENTS. ADJUSTMENTS MAY INCLUDE BUT ARE NOT LIMITED TO MANHOLE COVERS, VALVES, FIRE HYDRANTS, STORM DRAIN INLETS, STREET LIGHTS, TRAFFIC SIGNALS STRUCTURES, UTILITY POLES, SIDEWALKS, BURIED UTILITY CONDUIT AND PIPES

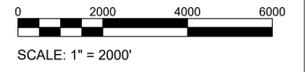
SEQUENCE OF CONSTRUCTION

PRELIMINARY		
1. HOLD PRECONSTRUCTION MEETING ONSITE WITH CITY OF MOUNT RAINIER. NOTE: PRIOR TO ANY DEMOLITION THE LIMITS OF DISTURBANCE (LOD) MUST BE CLEARLY MARKED	1 DAY	
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES EXCEPT FOR THE SILT FENCE ON PAVEMENT IN THE PHASE 3 WORK AREA. INSTALL TRAFFIC CONTROL SIGNAGE AND INSTALL TRAFFIC CONTROL DEVICES TO BLOCK ENTRANCE TO INTERSECTION FROM THE EAST.	1 DAY	
3. TEST PIT UTILITY LINES TO CONFIRM LOCATION AND DEPTH PER PLANS.	1 DAY	
PHASE 1		
1. INSTALL TRAFFIC AND PEDESTRAIN CONTROL DEVICES FOR PHASE 1 AREA.	1 DAY	
2. REMOVE EXISTING SIGN POST, DEMOLISH BOLLARDS AND ASPHALT PAVEMENT. INSTALL CONCRETE CURBS, ASPHALT PAVEMENT PATCHES, LANDSCAPE AREAS, SIGNS, AND BOLLARDS. DEMOLISH CONCRETE SIDEWALK, CONCRETE CURBS AND RAMP. INSTALL CONCRETE SIDEWALK AND RAMPS.	2 WEEKS	
3. REMOVE TRAFFIC CONTROL DEVICES.	1 DAY	
PHASE 2		
1. INSTALL TRAFFIC CONTROL DEVICES FOR PHASE 2 AREA.	1 DAY	
2. DEMOLISH BOLLARDS AND ASPHALT PAVEMENT. INSTALL ASPHALT PAVEMENT PATCHES.	2 DAYS	
3. REMOVE TRAFFIC CONTROL DEVICES.	1 DAY	
PHASE 3		
1. INSTALL TRAFFIC CONTROL DEVICES FOR PHASE 3 AREA.	1 DAY	
2. INSTALL SILT FENCE ON PAVEMENT IN PHASE 3 WORK AREA.	1 DAY	
3. DEMOLISH ASPHALT PAVEMENT. INSTALL CURB, LANDSCAPE AREA, AND SIGN.	1 WEEK	
4. REMOVE TRAFFIC CONTROL DEVICES.	1 DAY	
FINAL		
1. STABILIZE ALL DISTURBED AREAS, REMOVE SEDIMENT CONTROL DEVICES, REPAIR ANY DAMAGE TO SIDEWALKS, CURBS, OR ROADWAYS. INSTALL LANDSCAPE PLANTINGS.	1 DAY	
2. MILL AND OVERLAY THE ROADWAY. INSTALL TRAFFIC CONTROL DEVICES PER CONTRACTOR SUBMITTAL.	1 WEEK	
3. PAINT ROAD STRIPING AND CROSSWALKS. INSTALL TRAFFIC CONTROL DEVICES AND STATION FLAGGERS AS NEEDED.	1 DAY	

NOTES:

1. TOPOGRAPHY AND SURVEY FOR STRUCTURES AND SPOT ELEVATIONS PROVIDED BY JOHNSON BERNAT ASSOCIATES, INC. DATED: MAY 2022. HORIZONTAL DATUM: MARYLAND STATE PLANE NAD 83. VERTICAL DATUM: NAVD 88. ADDITIONAL TOPOGRAPHY FROM PG GIS DATED: 2018
 2. LIMIT OF DISTURBANCE: 4,902 SF
 3. CUT: 20 CY
 4. FILL: 55 CY
 5. SPOIL: 20 CY
 6. BORROW: 55 CY
 7. SPOIL TO BE DISPOSED OF AT AN APPROVED LANDFILL OR DISPOSAL SITE
 8. ALL FILL SOIL IS TO BE BORROWED. NO CUT SOIL IS TO BE USED AS FILL.
- TAX ID: ROW
ADDRESS: PERRY ST. AND 31ST ST. MOUNT RAINIER, MD 20712
PARCEL/SITE AREA: ROW
TAX ACCOUNT #: ROW
EXISTING ZONING: ROW
WSSC GRID: 205NE03
MAX CUT/FILL DEPTH: 2 FT
EXISTING IMPERVIOUS COVER: 0.11 ACRES
WATERSHED NAME AND #: UPPER ANACOSTIA WATERSHED, DNR 12-DIGIT DESIGNATOR 021402050808, MDE 8-DIGIT 02140205
NRI #: N/A
WCO #: N/A
TMDLS: NITROGEN, PHOSPHOROUS, TSS, BOD, PCBs AND BACTERIA
HIGHLY ERODIBLE SOILS: N/A
STEEP SLOPES: N/A
TIER II WATERSHED: N/A
100 YEAR FLOODPLAIN: N/A

LOW IMPACT DEVELOPMENT CENTER
5000 Sunnyside Avenue, Suite 100, Beltsville, MD 20705
Tel. (301) 982-5559
Fax. (301) 982-9305
www.lowimpactdevelopment.org



MISS UTILITY NOTE
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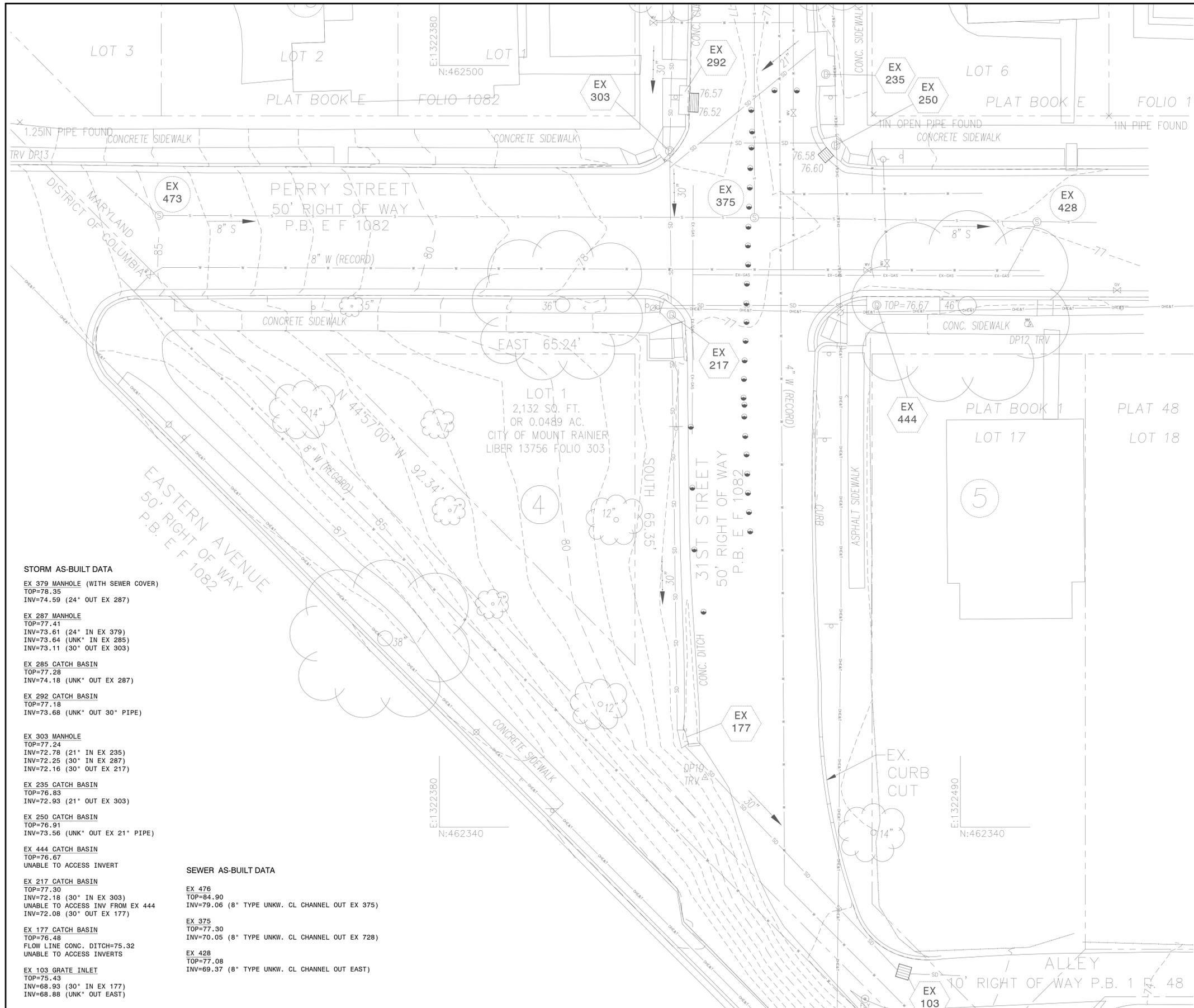
REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

COVER SHEET

31st ST. AND PERRY ST. INTERSECTION

MOUNT RAINIER, MD 20712
DISTRICT 2
PRINCE GEORGE'S COUNTY, MARYLAND

ISSUE:	DATE: 08/26/2024
SCALE: 1"=2000'	SHEET: 1 OF 14
FILE NO:	C-0.00
DRAFTED: GK	
CHECKED: DM	



- LEGEND:**
- EX. ROADS
 - EX. BUILDING
 - EX. CURB
 - EX. GUTTER
 - EX. PROPERTY LINE
 - EX. SIDEWALK
 - EX. HANDRAIL
 - EX. WIRE FENCE
 - SD— EX. STORM DRAIN
 - OHE&T— EX. OVERHEAD UTILITY
 - 21— EX. CONTOUR
 - S— EX. SANITARY SEWER LINE
 - W— EX. UNDERGROUND WATER
 - EX.GAS— EX. GAS
 - ⊙ EX. UTILITY POLE
 - ⊙ EX. BOLLARD
 - ⊙ EX. WATER MANHOLE
 - ⊙ EX. SANITARY SEWER MANHOLE
 - ⊙ EX. STORM DRAIN CURB INLET
 - ⊙ EX. STORM DRAIN GRATE INLET
 - ⊙ EX. SIGN
 - ⊙ EX. FIRE HYDRANT
 - ⊙ EX. WATER VALVE
 - ⊙ EX. GAS VALVE
 - ⊙ EX. STORM DRAIN MANHOLE
 - ⊙ EX. TREE
 - ⊙ EX. SANITARY CLEANOUT
 - ⊙ EX. WATER METER

- STORM AS-BUILT DATA**
- EX 379 MANHOLE (WITH SEWER COVER)
TOP=78.35
INV=74.59 (24" OUT EX 287)
 - EX 287 MANHOLE
TOP=77.41
INV=73.61 (24" IN EX 379)
INV=73.64 (UNK" IN EX 285)
INV=73.11 (30" OUT EX 303)
 - EX 285 CATCH BASIN
TOP=77.28
INV=74.18 (UNK" OUT EX 287)
 - EX 292 CATCH BASIN
TOP=77.18
INV=73.68 (UNK" OUT 30" PIPE)
 - EX 303 MANHOLE
TOP=77.24
INV=72.78 (21" IN EX 235)
INV=72.25 (30" IN EX 287)
INV=72.16 (30" OUT EX 217)
 - EX 235 CATCH BASIN
TOP=76.83
INV=72.93 (21" OUT EX 303)
 - EX 250 CATCH BASIN
TOP=76.91
INV=73.56 (UNK" OUT EX 21" PIPE)
 - EX 444 CATCH BASIN
TOP=76.67
UNABLE TO ACCESS INVERT
 - EX 217 CATCH BASIN
TOP=77.30
INV=72.18 (30" IN EX 303)
UNABLE TO ACCESS INV FROM EX 444
INV=72.08 (30" OUT EX 177)
 - EX 177 CATCH BASIN
TOP=76.48
FLOW LINE CONC. DITCH=75.32
UNABLE TO ACCESS INVERTS
 - EX 103 GRATE INLET
TOP=75.43
INV=68.93 (30" IN EX 177)
INV=68.88 (UNK" OUT EAST)

- SEWER AS-BUILT DATA**
- EX 476
TOP=84.90
INV=79.06 (8" TYPE UNKW. CL CHANNEL OUT EX 375)
 - EX 375
TOP=77.30
INV=70.05 (8" TYPE UNKW. CL CHANNEL OUT EX 728)
 - EX 428
TOP=77.08
INV=69.37 (8" TYPE UNKW. CL CHANNEL OUT EAST)

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REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

EXISTING CONDITIONS PLAN

31st ST. AND PERRY ST. INTERSECTION

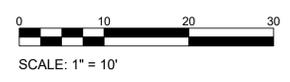
MOUNT RAINIER, MD 20712
DISTRICT 2
PRINCE GEORGE'S COUNTY, MARYLAND

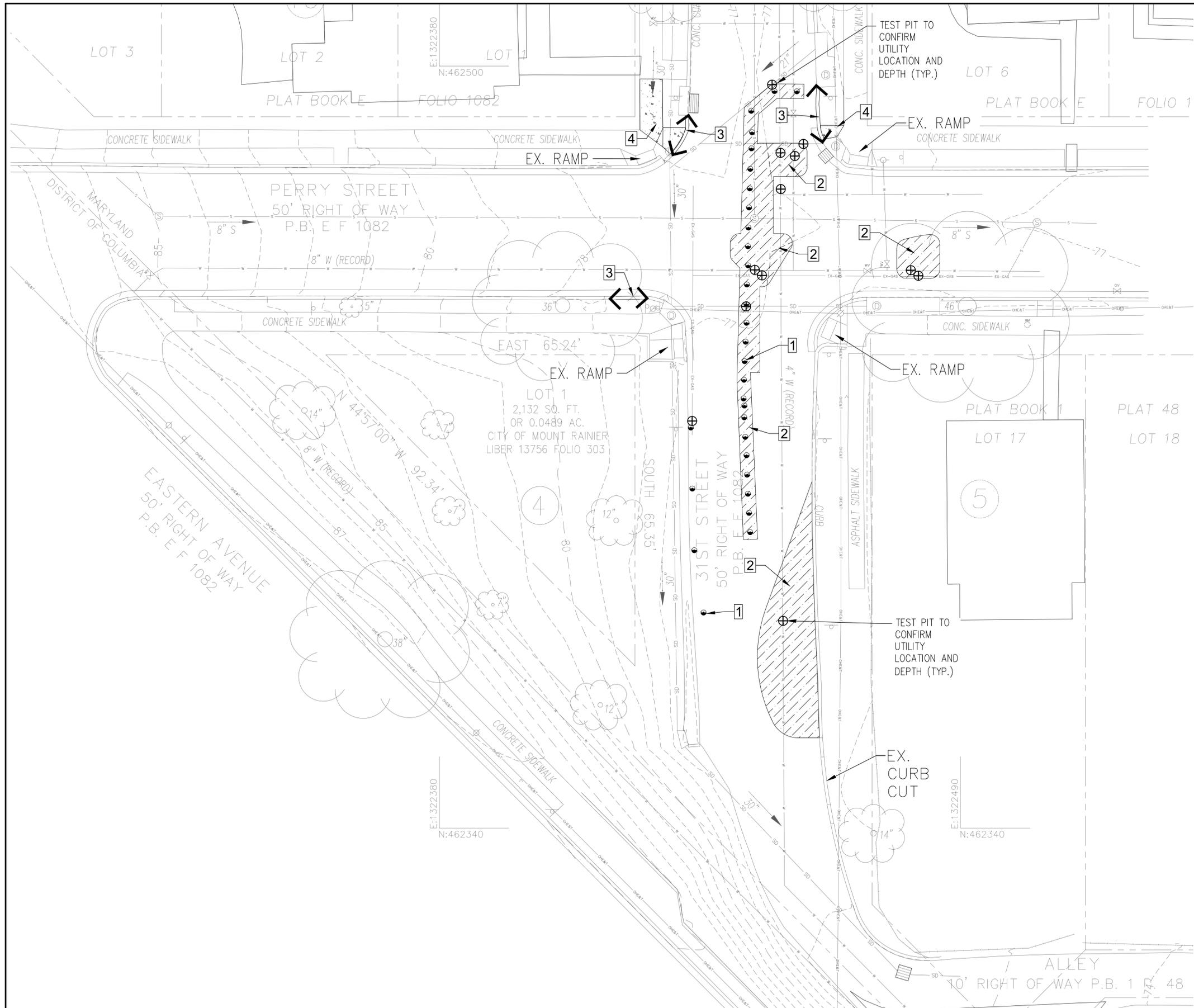
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Suite 100
Beltsville, MD 20705

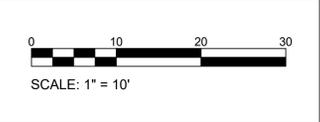
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- LEGEND:**
- EX. ROADS
 - EX. BUILDING
 - EX. CURB
 - EX. GUTTER
 - - - EX. PROPERTY LINE
 - - - EX. SIDEWALK
 - - - EX. HANDRAIL
 - - - EX. WIRE FENCE
 - SD- EX. STORM DRAIN
 - OHE&T- EX. OVERHEAD UTILITY
 - 21- EX. CONTOUR
 - S- EX. SANITARY SEWER LINE
 - W- EX. UNDERGROUND WATER
 - EX.GAS- EX. GAS
 - ⊙ EX. UTILITY POLE
 - ⊙ EX. BOLLARD
 - ⊙ EX. WATER MANHOLE
 - ⊙ EX. SANITARY SEWER MANHOLE
 - ⊙ EX. STORM DRAIN CURB INLET
 - ⊙ EX. STORM DRAIN GRATE INLET
 - ⊙ EX. SIGN
 - ⊙ EX. FIRE HYDRANT
 - ⊙ EX. WATER VALVE
 - ⊙ EX. GAS VALVE
 - ⊙ EX. STORM DRAIN MANHOLE
 - ⊙ EX. TREE
 - ⊙ EX. SANITARY CLEANOUT
 - ⊙ EX. WATER METER
 - ▨ ASPHALT REMOVAL
 - ▨ CONCRETE REMOVAL
 - ↔ CURB REMOVAL LIMITS

- KEYNOTES:**
- 1 EXISTING BOLLARD AND CONCRETE FOOTER TO BE REMOVED (TYP.). REMOVE ASPHALT AS NEEDED TO FACILITATE BOLLARD REMOVAL. REMOVE ANY SIGNS ATTACHED TO BOLLARDS
 - 2 EXISTING ASPHALT TO BE REMOVED. REMOVE 12" OF SOIL UNDER PAVEMENT WHERE PROP. LANDSCAPE AREAS ARE TO BE INSTALLED. REFER TO SHEET C-1.04 FOR LOCATIONS.
 - 3 EXISTING CURB TO BE REMOVED FOR RAMP CONSTRUCTION
 - 4 EXISTING CONCRETE SIDEWALK TO BE REMOVED



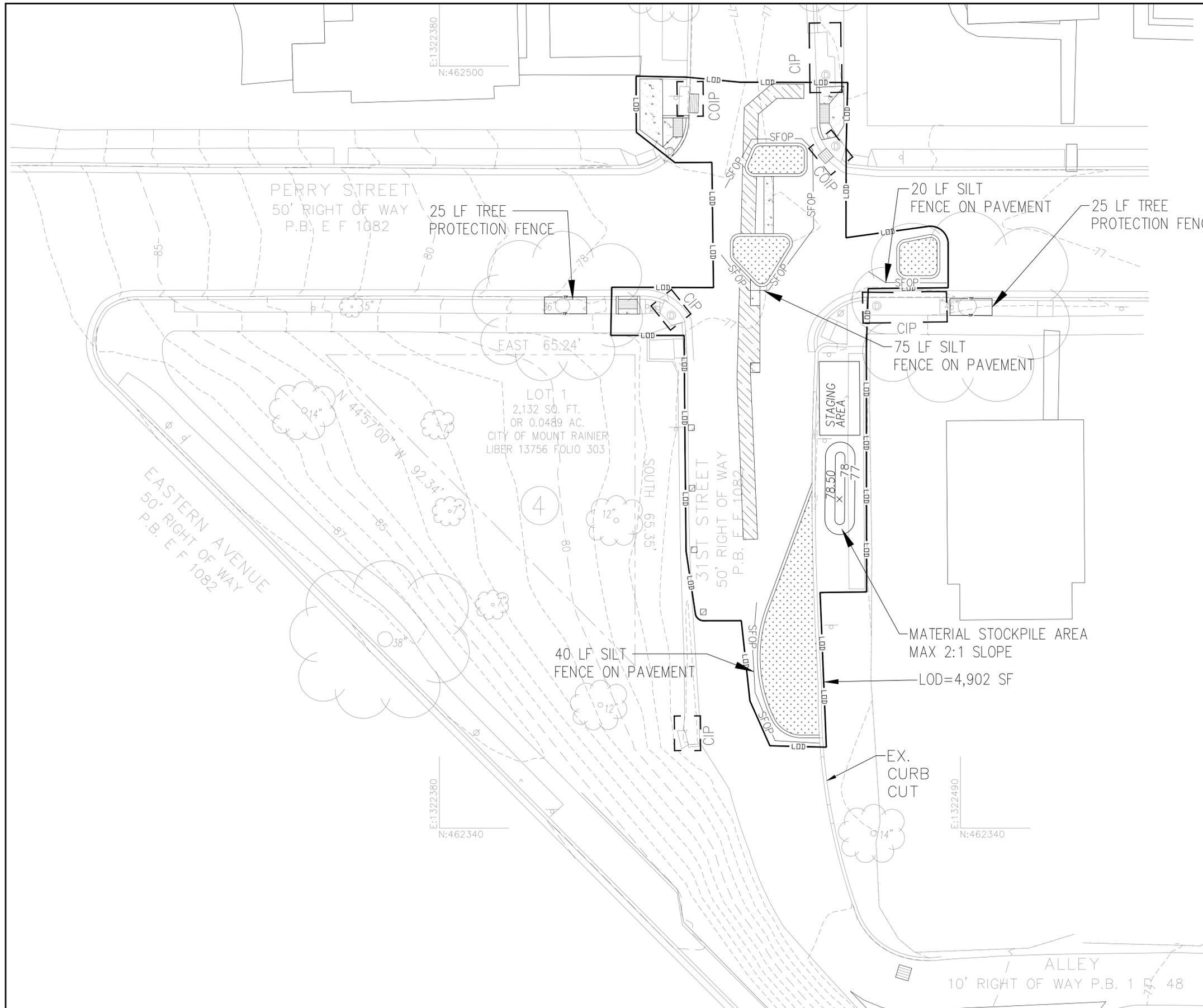
MISS UTILITY NOTE
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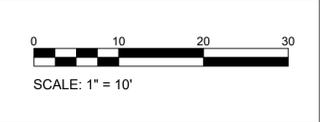
DEMOLITION PLAN

31st ST. AND PERRY ST. INTERSECTION
 MOUNT RAINIER, MD 20712
 DISTRICT 2
 PRINCE GEORGE'S COUNTY, MARYLAND

ISSUE:	DATE: 08/26/2024
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- LEGEND:**
- EX. ROADS
 - EX. BUILDING
 - EX. CURB
 - EX. GUTTER
 - - - EX. PROPERTY LINE
 - - - EX. SIDEWALK
 - - - EX. HANDRAIL
 - - - EX. WIRE FENCE
 - SD- EX. STORM DRAIN
 - OHE&T- EX. OVERHEAD UTILITY
 - Z- EX. CONTOUR
 - S- EX. SANITARY SEWER LINE
 - W- EX. UNDERGROUND WATER
 - EX.GAS- EX. GAS
 - ⊗ EX. UTILITY POLE
 - EX. BOLLARD
 - ⊙ EX. WATER MANHOLE
 - ⊙ EX. SANITARY SEWER MANHOLE
 - ⊙ EX. STORM DRAIN CURB INLET
 - ⊙ EX. STORM DRAIN GRATE INLET
 - ⊙ EX. SIGN
 - ⊙ EX. FIRE HYDRANT
 - ⊙ EX. WATER VALVE
 - ⊙ EX. GAS VALVE
 - ⊙ EX. STORM DRAIN MANHOLE
 - ⊙ EX. TREE
 - ⊙ EX. SANITARY CLEANOUT
 - ⊙ EX. WATER METER
 - ▨ PROP. LANDSCAPE AREA
 - ▨ PROP. CONCRETE
 - ▨ PROP. ASPHALT PATCH
 - LOD STOCKPILE AREA
 - ⊙ CIP CURB INLET PROTECTION
 - ⊙ COP COMBINATION INLET PROTECTION
 - SFOP- SILT FENCE ON PAVEMENT
 - TF- TREE PROTECTION FENCE



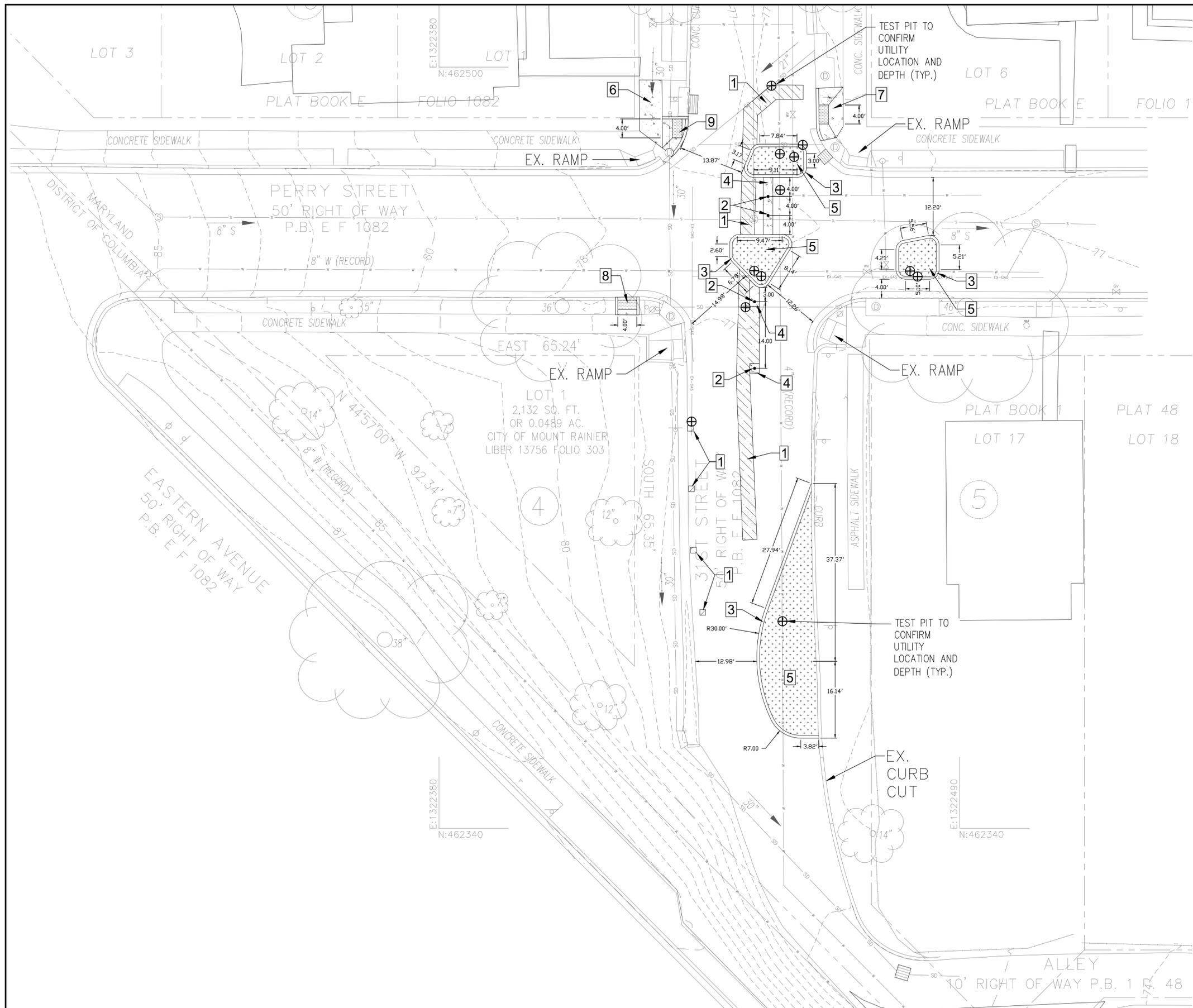
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REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

EROSION AND SEDIMENT CONTROL PLAN

31st ST. AND PERRY ST. INTERSECTION
 MOUNT RAINIER, MD 20712
 DISTRICT 2
 PRINCE GEORGE'S COUNTY, MARYLAND

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LEGEND:

- EX. ROADS
- EX. BUILDING
- EX. CURB
- EX. GUTTER
- - - EX. PROPERTY LINE
- - - EX. SIDEWALK
- - - EX. HANDRAIL
- - - EX. WIRE FENCE
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- ⊙ EX. SIGN
- ⊙ EX. FIRE HYDRANT
- ⊙ EX. WATER VALVE
- ⊙ EX. GAS VALVE
- ⊙ EX. STORM DRAIN MANHOLE
- ⊙ EX. TREE
- ⊙ EX. SANITARY CLEANOUT
- ⊙ EX. WATER METER
- ▨ PROP. LANDSCAPE AREA
- ▨ PROP. CONCRETE
- ▨ PROP. ASPHALT PATCH
- PROP. BOLLARD

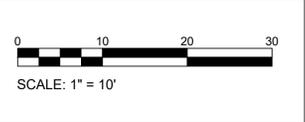
KEYNOTES:

- 1 ASPHALT PATCH TO MATCH EX. GRADES.
- 2 PROP. MAXI FORCE COLLAPSIBLE BOLLARD. SEE DETAIL MCSP-SS1-S ON C-1.09.
- 3 PROP. 6" MODIFIED CONCRETE CURB WITHOUT GUTTER. SEE DETAIL STD. 300.07 ON SHEET C-1.08.
- 4 PROP. 2' WIDE BOLLARD CONCRETE FOOTER. SEE DETAIL MCSP-SS1-S ON C-1.09.
- 5 PROP. LANDSCAPED AREA
- 6 PROP. CONCRETE SIDEWALK. REFER TO DETAIL STD. 300.05 ON SHEET C-1.08.
- 7 PROP. HANDICAP RAMP. SEE DETAIL STD. 300.01 ON SHEET C-1.08 FOR REFERENCE.
- 8 PROP. HANDICAP RAMP WITH RETURNED CURB EDGES. SEE DETAIL ENG-1 ON SHEET C-1.08.
- 9 PROP. HANDICAP RAMP WITH SINGLE SIDED RETURNED CURB EDGE. SEE DETAIL ENG-1 ON SHEET C-1.08.

NOTES:

1. ALL CURB RADII ARE 2 FT UNLESS OTHERWISE NOTED
2. BOLLARDS IN CENTER OF INTERSECTION TO FOLD EAST.
3. BOLLARDS SOUTH OF MEDIAN TO FOLD SOUTH.

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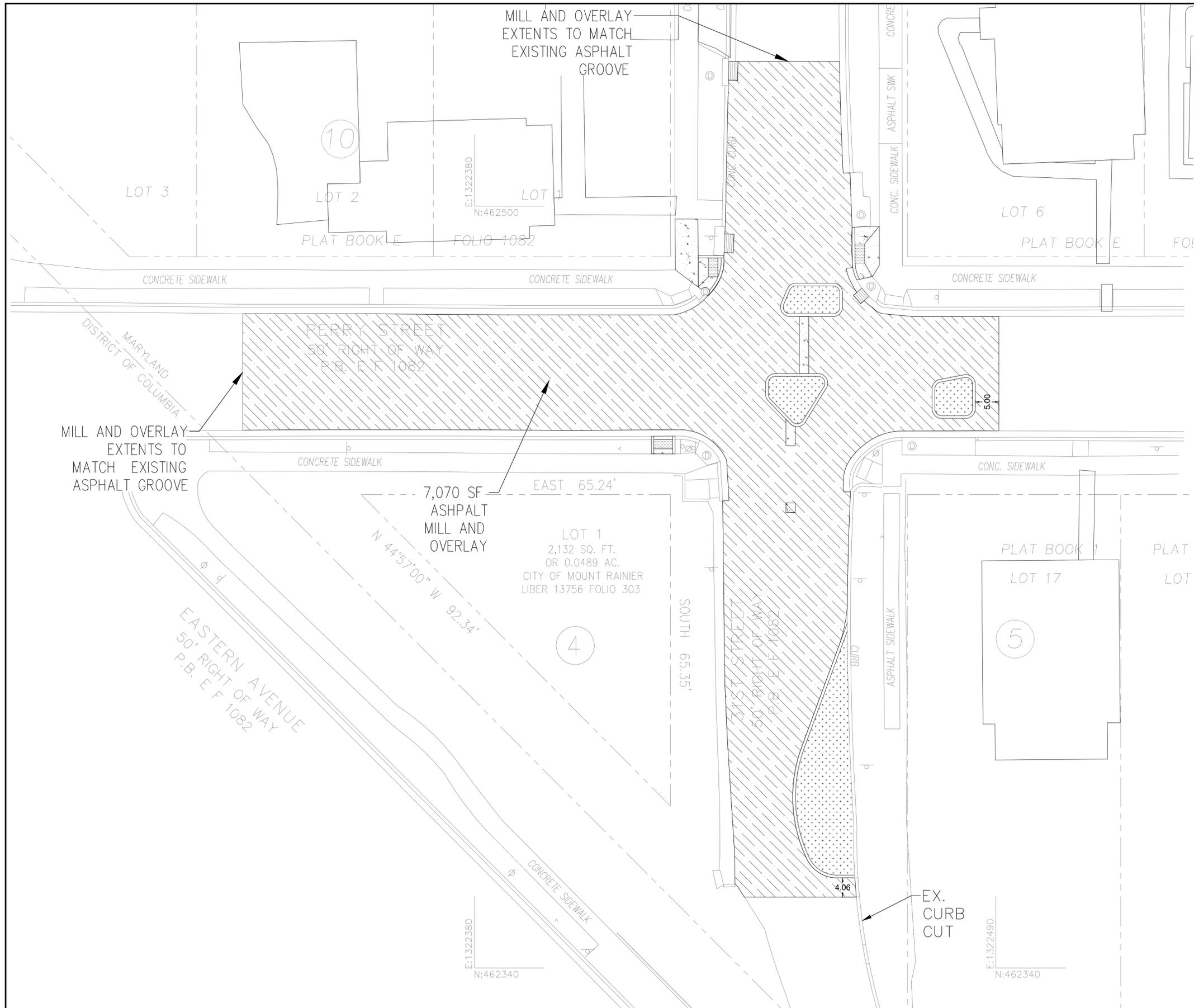
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REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

SITE PLAN

31st ST. AND PERRY ST. INTERSECTION
 MOUNT RAINIER, MD 20712
 DISTRICT 2
 PRINCE GEORGE'S COUNTY, MARYLAND

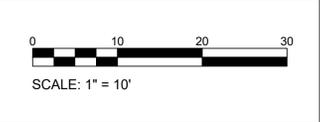
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- LEGEND:**
- EX. ROADS
 - EX. BUILDING
 - EX. CURB
 - EX. GUTTER
 - - - EX. PROPERTY LINE
 - EX. SIDEWALK
 - - - EX. HANDRAIL
 - - - EX. WIRE FENCE
 - SD— EX. STORM DRAIN
 - OHE&T— EX. OVERHEAD UTILITY
 - Z- EX. CONTOUR
 - S- EX. SANITARY SEWER LINE
 - W- EX. UNDERGROUND WATER
 - EX.GAS- EX. GAS
 - ⊙ EX. UTILITY POLE
 - EX. BOLLARD
 - ⊙ W EX. WATER MANHOLE
 - ⊙ SS EX. SANITARY SEWER MANHOLE
 - ⊙ SD EX. STORM DRAIN CURB INLET
 - ⊙ EX. STORM DRAIN GRATE INLET
 - ⊙ EX. SIGN
 - ⊙ EX. FIRE HYDRANT
 - ⊙ EX. WATER VALVE
 - ⊙ EX. GAS VALVE
 - ⊙ EX. STORM DRAIN MANHOLE
 - ⊙ EX. TREE
 - ⊙ EX. SANITARY CLEANOUT
 - ⊙ EX. WATER METER
 - ▨ PROP. LANDSCAPE AREA
 - ▨ PROP. CONCRETE
 - ▨ PROP. ASPHALT MILL AND OVERLAY

- NOTES:**
1. ALL SITE WORK EXCLUDING ROAD STRIPING MUST BE COMPLETED PRIOR TO MILL AND OVERLAY WORK.
 2. THE SITE MUST BE FULLY STABILIZED AND ESC CONTROLS MUST BE REMOVED PRIOR TO MILL AND OVERLAY WORK.
 3. CONTRACTOR TO PROVIDE SUBMITTAL FOR TRAFFIC CONTROL PLAN DURING MILL AND OVERLAY WORK.

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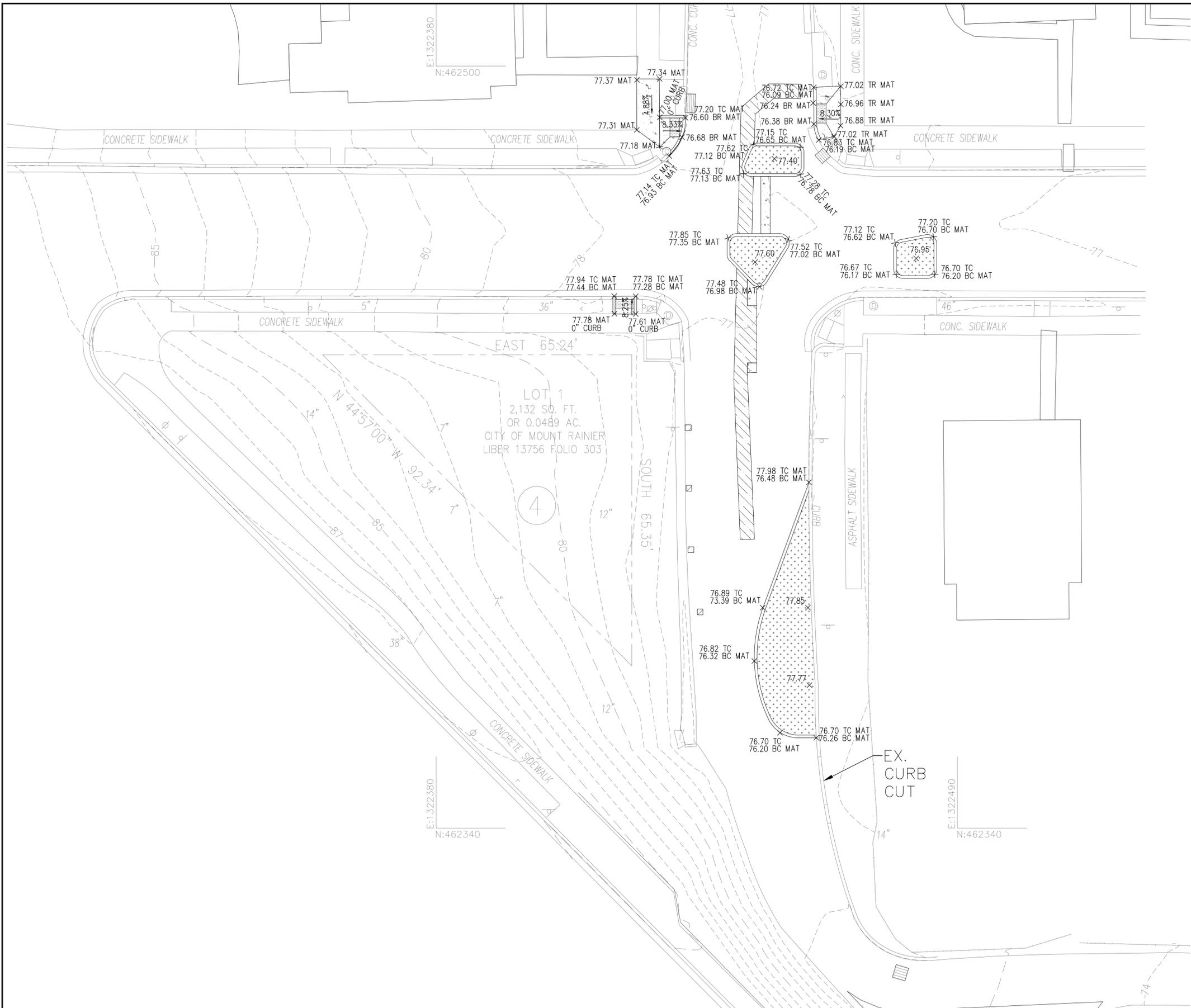
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REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

PAVEMENT RESTORATION PLAN

31st ST. AND PERRY ST. INTERSECTION
 MOUNT RAINIER, MD 20712
 DISTRICT 2
 PRINCE GEORGE'S COUNTY, MARYLAND

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NAD-83
 NAD-88

LEGEND:

- EX. ROADS
- EX. BUILDING
- EX. CURB
- EX. GUTTER
- - - EX. PROPERTY LINE
- - - EX. SIDEWALK
- - - EX. HANDRAIL
- - - EX. WIRE FENCE
- SD- EX. STORM DRAIN
- OHE&T- EX. OVERHEAD UTILITY
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- ⊙ EX. FIRE HYDRANT
- ⊙ EX. WATER VALVE
- ⊙ EX. GAS VALVE
- ⊙ EX. STORM DRAIN MANHOLE
- ⊙ EX. TREE
- ⊙ EX. SANITARY CLEANOUT
- ⊙ EX. WATER METER
- ⊙ PROP. LANDSCAPE AREA
- ⊙ PROP. CONCRETE
- ⊙ PROP. ASPHALT PATCH

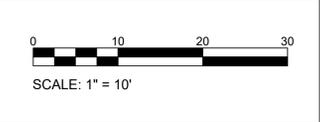
NOTES:

1. ENSURE RAMP SLOPES DO NOT EXCEED 8.33% PER ADA GUIDELINES

SPOT SHOT KEY:

- MAT MATCH EXISTING ELEVATION
- TC TOP OF CURB
- BC BOTTOM OF CURB

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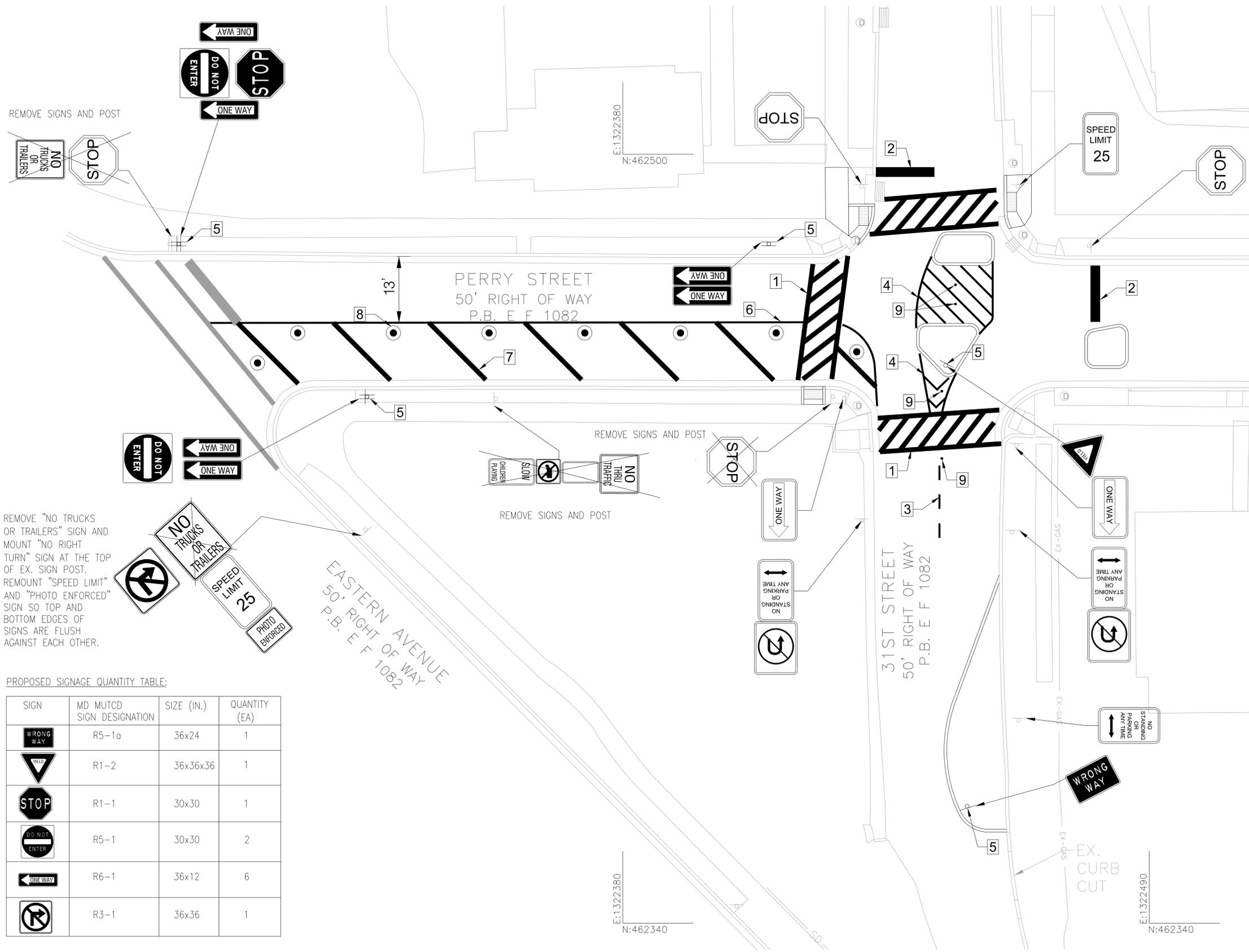
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GRADING PLAN

31st ST. AND PERRY ST. INTERSECTION
 MOUNT RAINIER, MD 20712
 DISTRICT 2
 PRINCE GEORGE'S COUNTY, MARYLAND

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- LEGEND:**
- EX. ROADS
 - EX. BUILDING
 - EX. CURB
 - EX. GUTTER
 - EX. PROPERTY LINE
 - EX. SIDEWALK
 - EX. HANDRAIL
 - EX. WIRE FENCE
 - SD— EX. STORM DRAIN
 - OHE&T— EX. OVERHEAD UTILITY
 - 21— EX. CONTOUR
 - S— EX. SANITARY SEWER LINE
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 - ⊙ EX. SIGN
 - ⊙ EX. FIRE HYDRANT
 - ⊙ EX. WATER VALVE
 - ⊙ EX. GAS VALVE
 - ⊙ EX. STORM DRAIN MANHOLE
 - ⊙ EX. TREE
 - ⊙ EX. SANITARY CLEANOUT
 - ⊙ EX. WATER METER
 - ⊙ PROP. SIGN
 - ⊙ PROP. BOLLARD
 - ⊙ PROP. FLEX POST

- NOTES:**
1. ALL STRIPING TO BE DONE AFTER MILL AND OVERLAY IS COMPLETED.
 2. ALL LANE LINES SHALL BE DEMARCATED WITH EXTRUDED THERMOPLASTIC MATERIAL AND BE APPLIED BY TRUCK MOUNTED EQUIPMENT. CROSSWALKS AND INTERSECTION STOP BARS CAN EITHER BE EXTRUDED OR PRE-CUT HEAT APPLIED THERMOPLASTIC MATERIAL.
 3. ALL PROPOSED SIGNS SHALL HAVE A 7' MINIMUM VERTICAL CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE GROUND LEVEL.

- KEYNOTES:**
- 1] PROP. CROSSWALK. SEE MD MUTCD FIGURE 3B-20a ON SHEET C-1.09.
 - 2] PROP. INTERSECTION STOP BAR. 24" WIDE SOLID WHITE PAVEMENT MARKING.
 - 3] PROP. LANE MARKINGS. MD MUTCD TYPE III 5" WIDE WHITE PAVEMENT MARKING, 3' SEGMENT - 3' GAP.
 - 4] PROP. NO DRIVE AREA. 5" WIDE WHITE PAVEMENT MARKINGS. 2' SPACING AT 45 DEGREE ANGLES.
 - 5] PROP. SIGN WITH GALVANIZED STEEL U CHANNEL POST. SIGN POST SET BACK 2' FROM CURB FACE.
 - 6] PROP. 5" WIDE YELLOW PAVEMENT MARKINGS.
 - 7] PROP. 12" WIDE YELLOW PAVEMENT MARKINGS. 20' SPACING AT 45 DEGREE ANGLES.
 - 8] PROP. FLEX POSTS. 20' TYPICAL SPACING.
 - 9] PROP. BOLLARDS. SEE DETAIL ON SHEET C-1.09.

REMOVE "NO TRUCKS OR TRAILERS" SIGN AND MOUNT "NO RIGHT TURN" SIGN AT THE TOP OF EX. SIGN POST. REMOUNT "SPEED LIMIT" AND "PHOTO ENFORCED" SIGN SO TOP AND BOTTOM EDGES OF SIGNS ARE FLUSH AGAINST EACH OTHER.

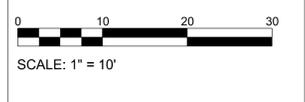
PROPOSED SIGNAGE QUANTITY TABLE:

SIGN	MD MUTCD SIGN DESIGNATION	SIZE (IN.)	QUANTITY (EA)
	R5-1a	36x24	1
	R1-2	36x36x36	1
	R1-1	30x30	1
	R5-1	30x30	2
	R6-1	36x12	6
	R3-1	36x36	1

REMOVE SIGNS AND POST

EASTERN AVENUE
50' RIGHT OF WAY
P.B. E F 1082

31ST STREET
50' RIGHT OF WAY
P.B. E F 1082



MISS UTILITY NOTE
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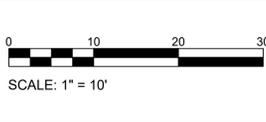
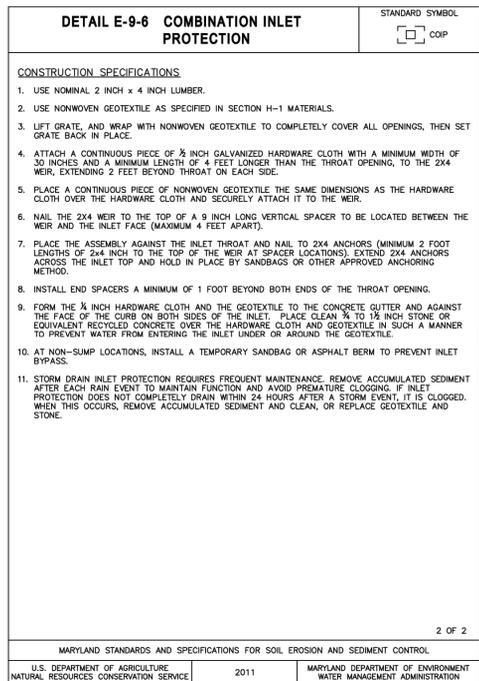
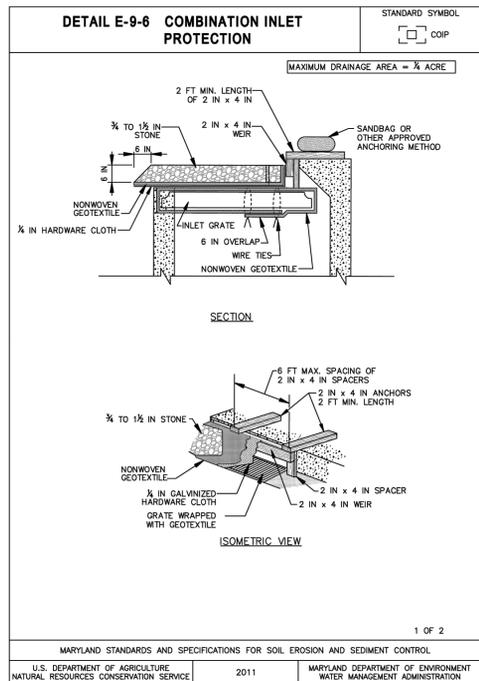
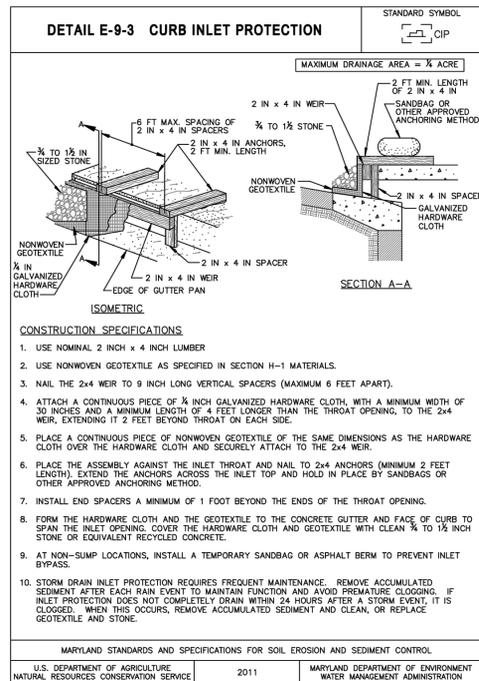
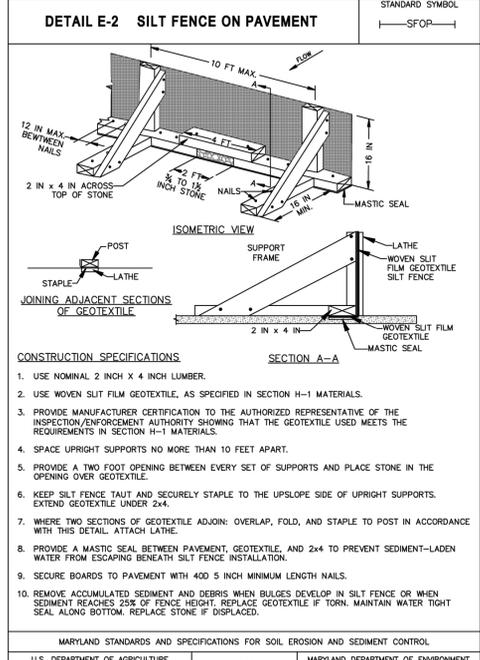
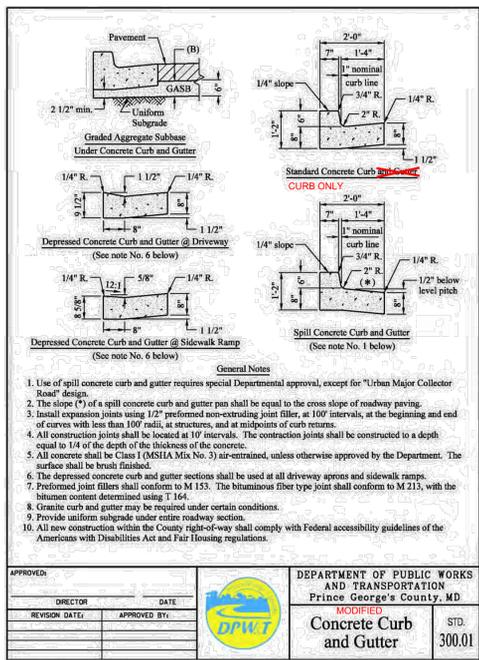
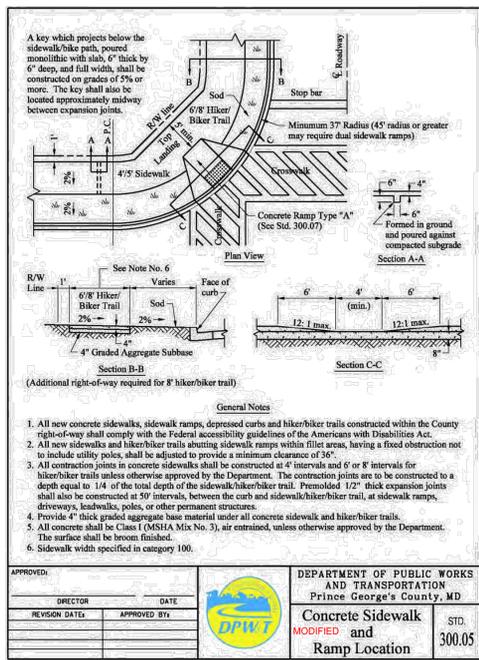
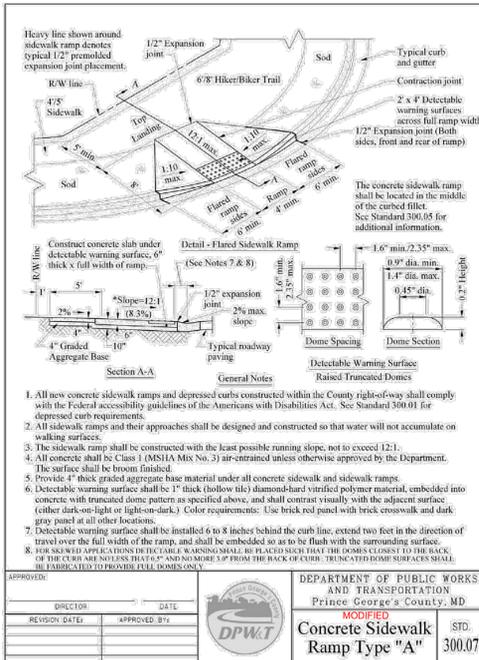
STRIPING AND SIGNAGE PLAN

31st ST. AND PERRY ST. INTERSECTION

MOUNT RAINIER, MD 20712
DISTRICT 2
PRINCE GEORGE'S COUNTY, MARYLAND

ISSUE:	DATE: 08/09/2024
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Tel. (301) 234-6234
www.idesigneng.com

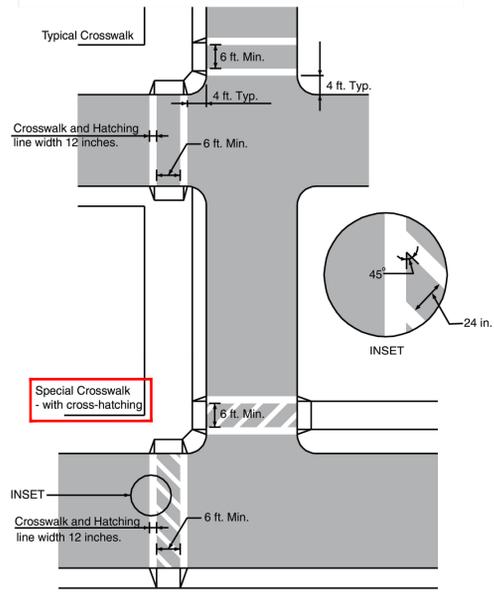


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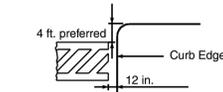
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Figure 3B-20a. Examples of Crosswalk and Crosswalk Lines

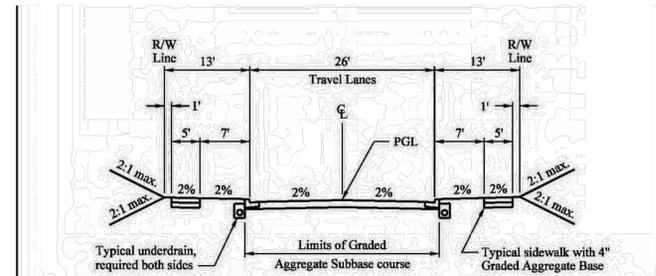


A crosswalk placed at an intersection is preferred to be placed no closer than 4 feet from the nearest edge of the intersecting roadway.

Across roadways at non-intersection locations, locations that are unexpected, school crossing, and across ramps, crosswalk lines shall be 12 inches wide. The space between the crosswalk lines shall be hatched with diagonal white lines that are 12 inches wide.



When using lane tape material, allow for gutter drainage by starting material 12 inches from the curb edge.



Design Speed	Subgrade (A)	Pavement Section				Right-of-Way Width
		GASB Course (B)	HMA Base Course (C)	Intermediate Surface Course (D)	Final Surface Course (E)	
30 MPH	CBR ≥ 7	6"	3"	1 1/2"	1 1/2"	50'

- Footnotes**
- The top 12" of in-situ subgrade material shall have a minimum California Bearing Ratio (CBR) value of 7. See Table I-3, I-4, I-5, I-6, I-7, I-8 and I-9 of Section I "Roadway Development Guidelines" for subgrade criteria.
 - Graded Aggregate Subbase (GASB). See Section II "Technical Specifications."
 - Superpave Mixture Hot Mix Asphalt (HMA) base, 19mm, PG 64-22. See Section II "Technical Specifications."
 - Superpave Mixture Hot Mix Asphalt (HMA) surface, 9.5mm, PG 64-22. See Section II "Technical Specifications."
 - Superpave Mixture Hot Mix Asphalt (HMA) surface, 9.5mm, PG 64-22. See Section II "Technical Specifications."

- General Notes**
- Travel lanes accommodate two (2) 11-foot minimum lanes, or one (1) 12-foot lane and parallel parking on both sides.
 - See Table I-2 of Section I "Roadway Development Guidelines" for design criteria.
 - See Section II "Technical Specifications" for materials and method of construction.
 - Refer to Standard 300.13 for underdrain details.
 - Refer to Standards 300.01 and 300.02 for curb and gutter details.
 - Refer to Standards 300.05 through 300.08 for sidewalk details.
 - Refer to Category 500 for street light standards.
 - Refer to Category 600 for landscaping within the County right-of-way.
 - All unpaved areas within the County right-of-way shall receive a minimum of 3" of topsoil and sod.
 - All new construction within the County right-of-way shall comply with Federal accessibility guidelines of the Americans with Disabilities Act, including acceptable sidewalk passing space requirements.
 - Top of curb elevation equals Profile Grade Line (PGL) elevation plus 0.14' for 6" curb.

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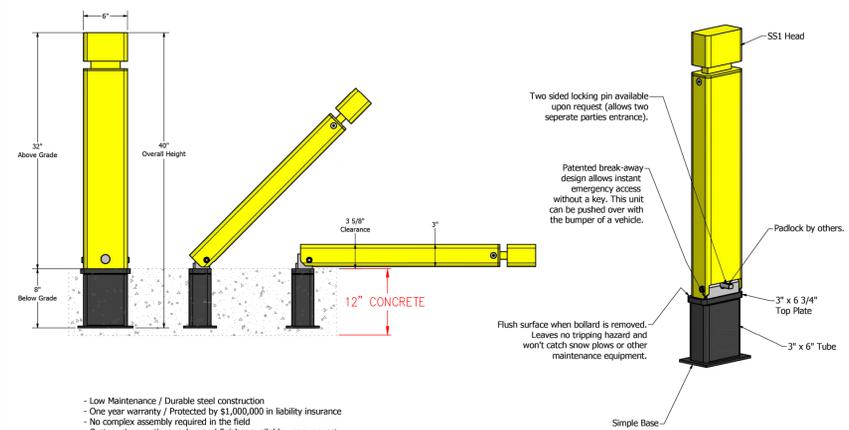
DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION
Prince George's County, MD

Urban Secondary Residential Road

STD. 100.07

MaxiForce™ Collapsible Bollard
Standard Body, Padlock Operated, Standard Style 1 Head, Simple Base

AHJ approved in many cities / counties / campuses for use in fire lanes per International Fire Code (IFC) 503



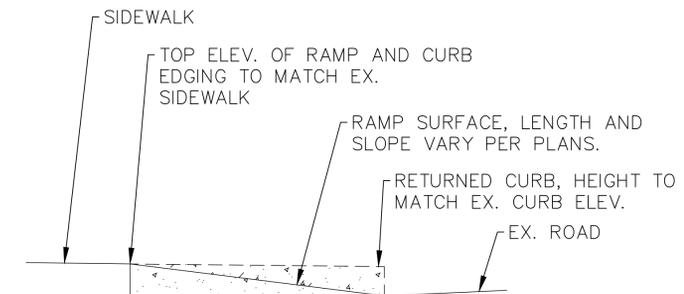
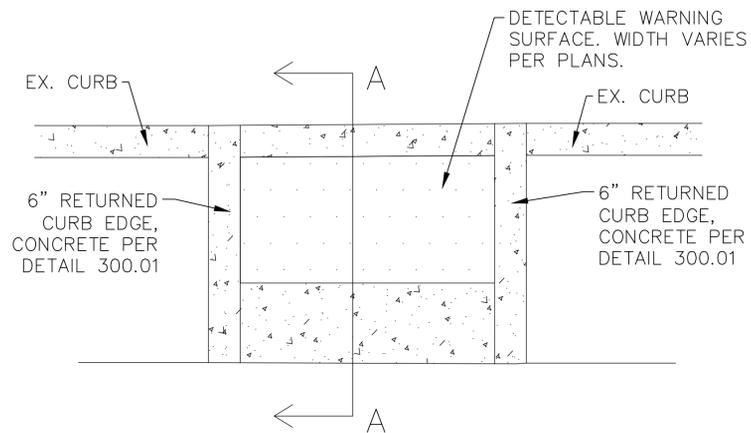
- Low Maintenance / Durable steel construction
- One year warranty / Protected by \$1,000,000 in liability insurance
- No complex assembly required in the field
- Custom sizes, options, colors and finishes available upon request
- Finish options available (add code to the end of the model number)
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 - Powder coated with DRYZINC primer (PCZ)
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ENG 1 RETURNED CURB HANDICAP RAMP TYPICAL DETAIL NOT TO SCALE



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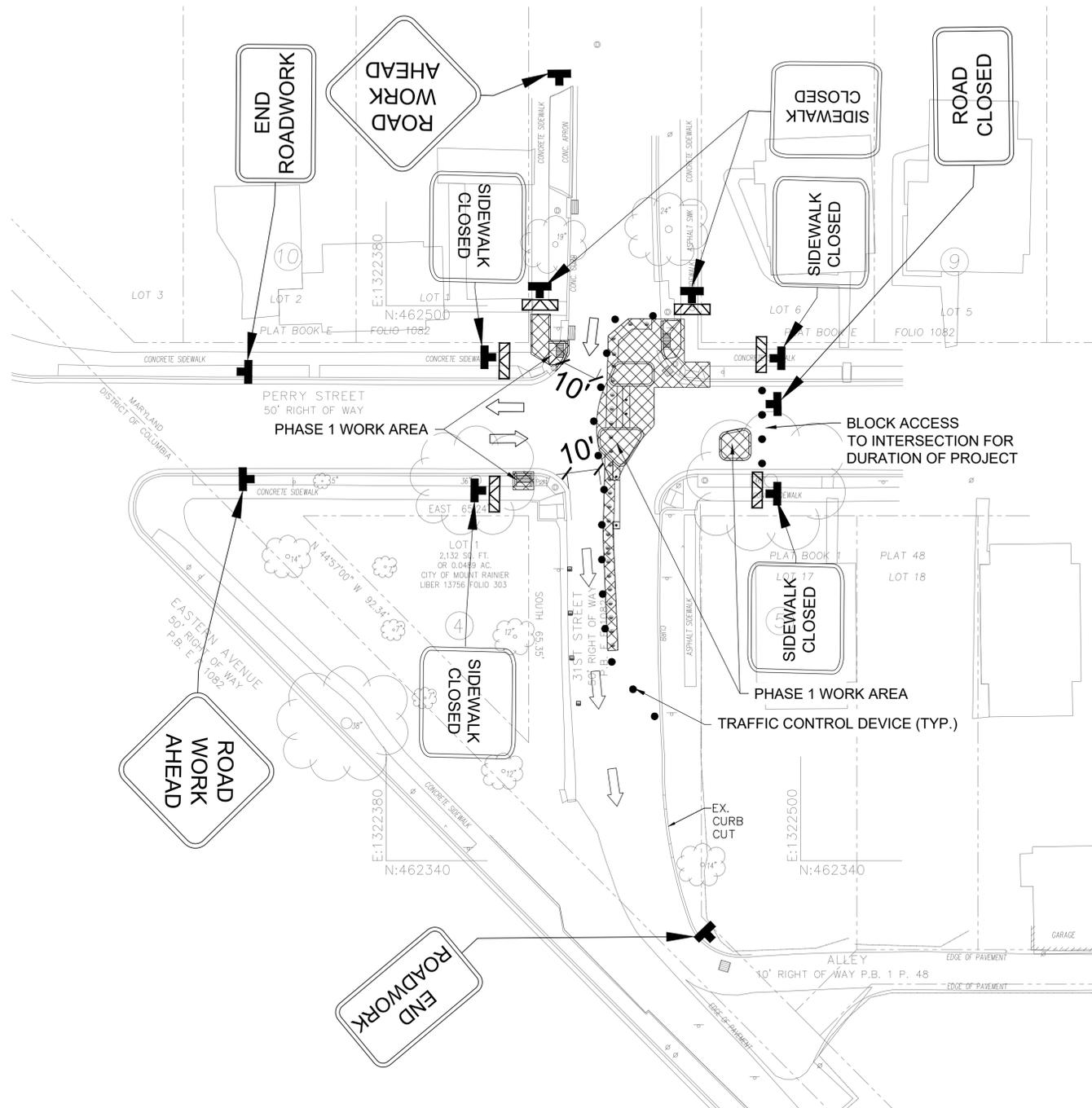
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DETAILS PLAN

31st ST. AND PERRY ST. INTERSECTION
MOUNT RAINIER, MD 20712
DISTRICT 2
PRINCE GEORGE'S COUNTY, MARYLAND

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NAD-83
NAYD-88

LEGEND:

- EX. ROADS
- EX. BUILDING
- EX. CURB
- EX. GUTTER
- - - EX. PROPERTY LINE
- EX. SIDEWALK
- - - EX. HANDRAIL
- - - EX. WIRE FENCE
- SD— EX. STORM DRAIN
- OHE&T— EX. OVERHEAD UTILITY
- Z1— EX. CONTOUR
- S— EX. SANITARY SEWER LINE
- W— EX. UNDERGROUND WATER
- EX.GAS— EX. GAS
- ⊙ EX. UTILITY POLE
- ⊙ EX. BOLLARD
- ⊙ EX. WATER MANHOLE
- ⊙ EX. SANITARY SEWER MANHOLE
- ⊙ EX. STORM DRAIN CURB INLET
- ⊙ EX. STORM DRAIN GRATE INLET
- ⊙ EX. SIGN
- ⊙ EX. FIRE HYDRANT
- ⊙ EX. WATER VALVE
- ⊙ EX. GAS VALVE
- ⊙ EX. STORM DRAIN MANHOLE
- ⊙ EX. TREE
- ⊙ EX. SANITARY CLEANOUT
- ⊙ EX. WATER METER
- ⊙ PROP. WORK AREA
- ⊙ PROP. PEDESTRIAN TRAFFIC BARRIER
- ⊙ PROP. TRAFFIC CONTROL SIGN
- ⊙ PROP. TRAFFIC CONTROL DEVICE
- ↔ TRAFFIC DIRECTION ARROW

NOTES:

1. ORANGE CONES OR 2' DIA. BARRELS MAY BE USED AS TRAFFIC CONTROL DEVICES.
2. SPACE TRAFFIC CONTROL DEVICES A MAXIMUM OF 10' APART
3. MAINTAIN 10' MINIMUM LANE WIDTH FOR BYPASS TRAFFIC



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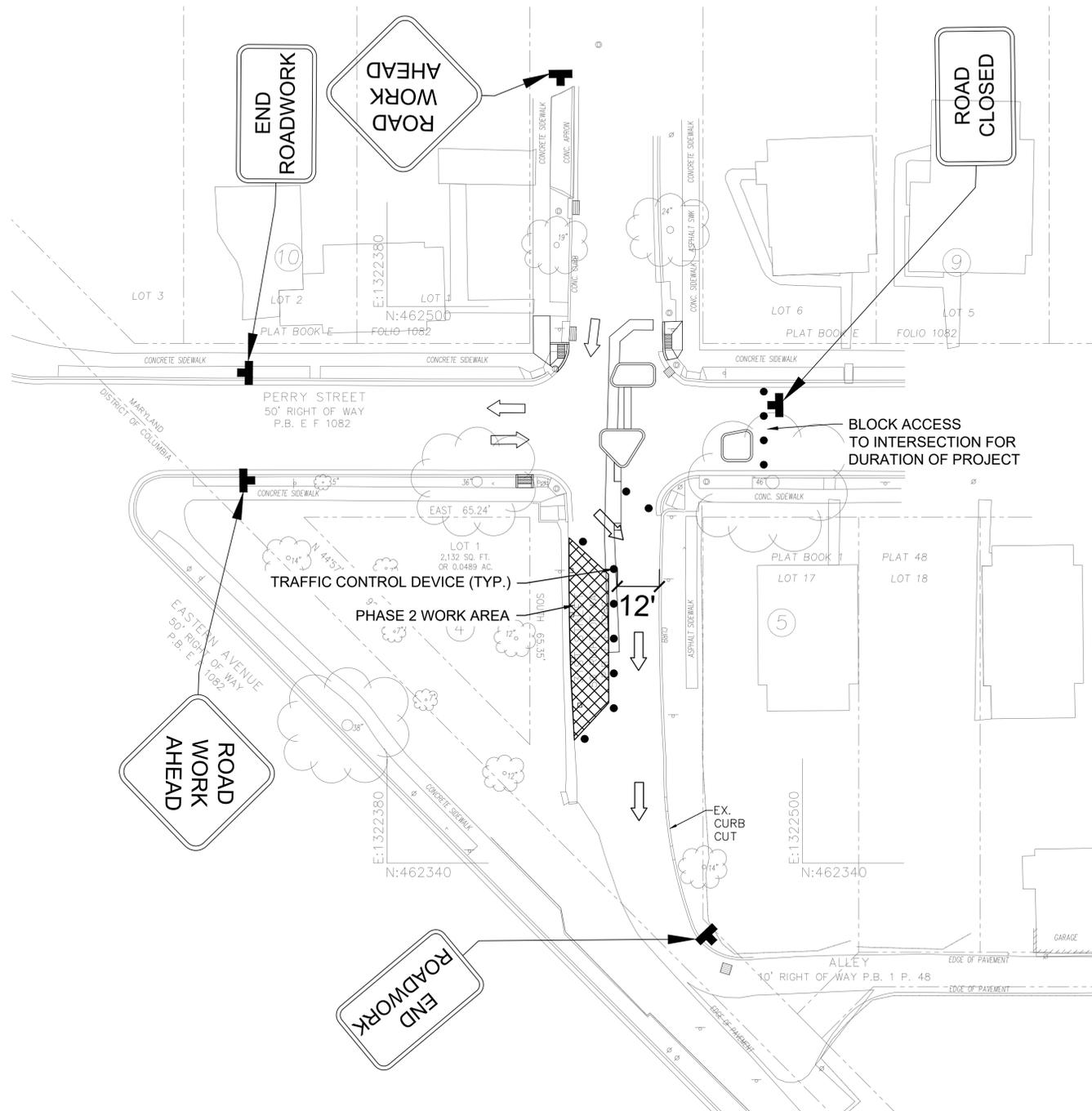
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TRAFFIC CONTROL PLAN

31st ST. AND PERRY ST. INTERSECTION

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LEGEND:

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---	EX. BUILDING
---	EX. CURB
---	EX. GUTTER
---	EX. PROPERTY LINE
---	EX. SIDEWALK
---	EX. HANDRAIL
---	EX. WIRE FENCE
SD	EX. STORM DRAIN
-OHE&T-	EX. OVERHEAD UTILITY
-21-	EX. CONTOUR
-S-	EX. SANITARY SEWER LINE
-W-	EX. UNDERGROUND WATER
-EX.GAS-	EX. GAS
⊙	EX. UTILITY POLE
●	EX. BOLLARD
⊙	EX. WATER MANHOLE
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⊙	EX. GAS VALVE
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⊙	EX. SANITARY CLEANOUT
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⊙	PROP. WORK AREA
⊙	PROP. PEDESTRIAN TRAFFIC BARRIER
⊙	PROP. TRAFFIC CONTROL SIGN
⊙	PROP. TRAFFIC CONTROL DEVICE
→	TRAFFIC DIRECTION ARROW

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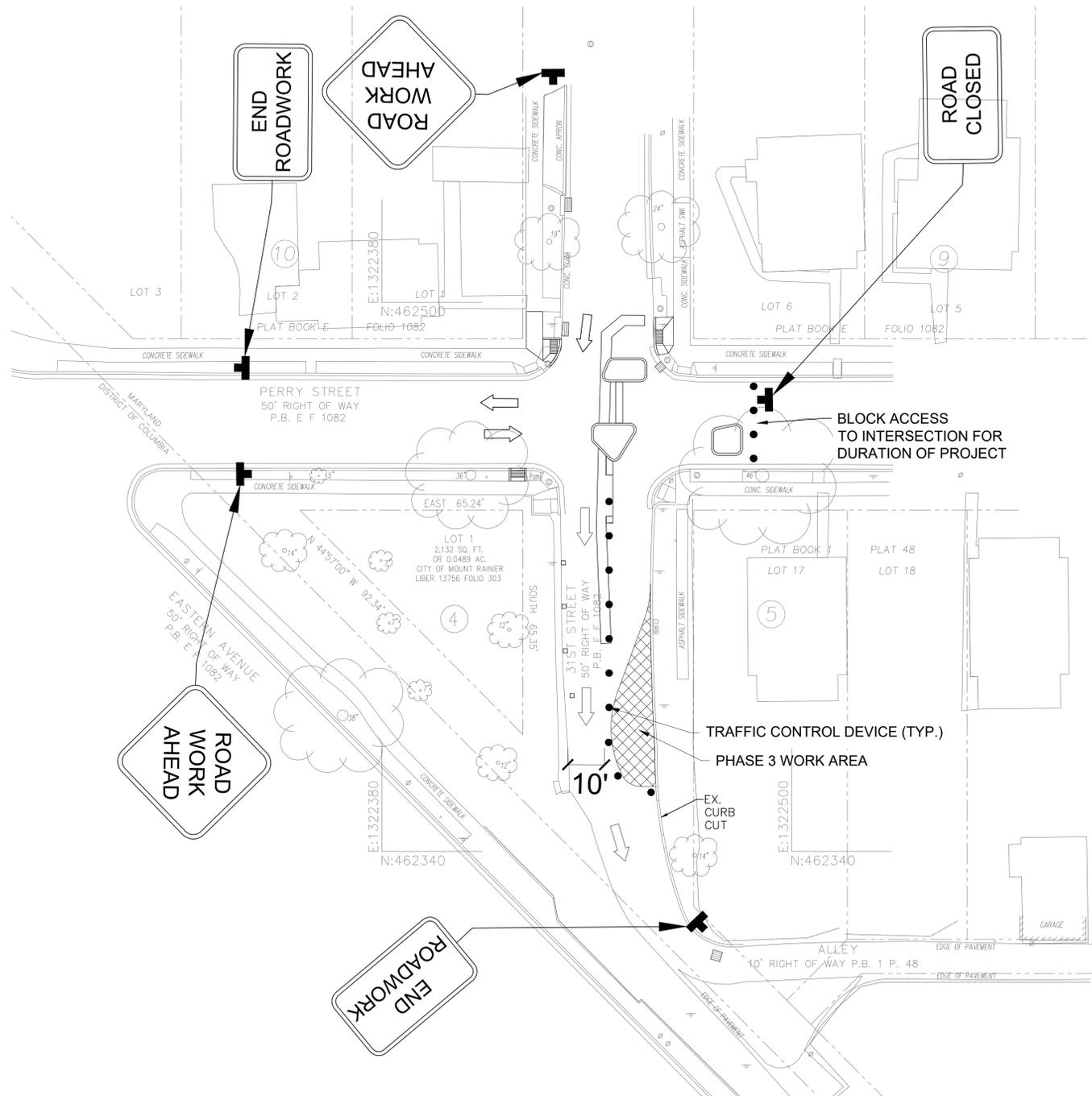
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TRAFFIC CONTROL PLAN

31st ST. AND PERRY ST. INTERSECTION

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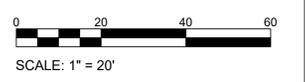
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- EX. GUTTER
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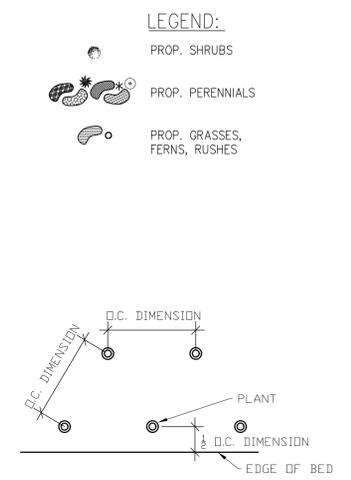
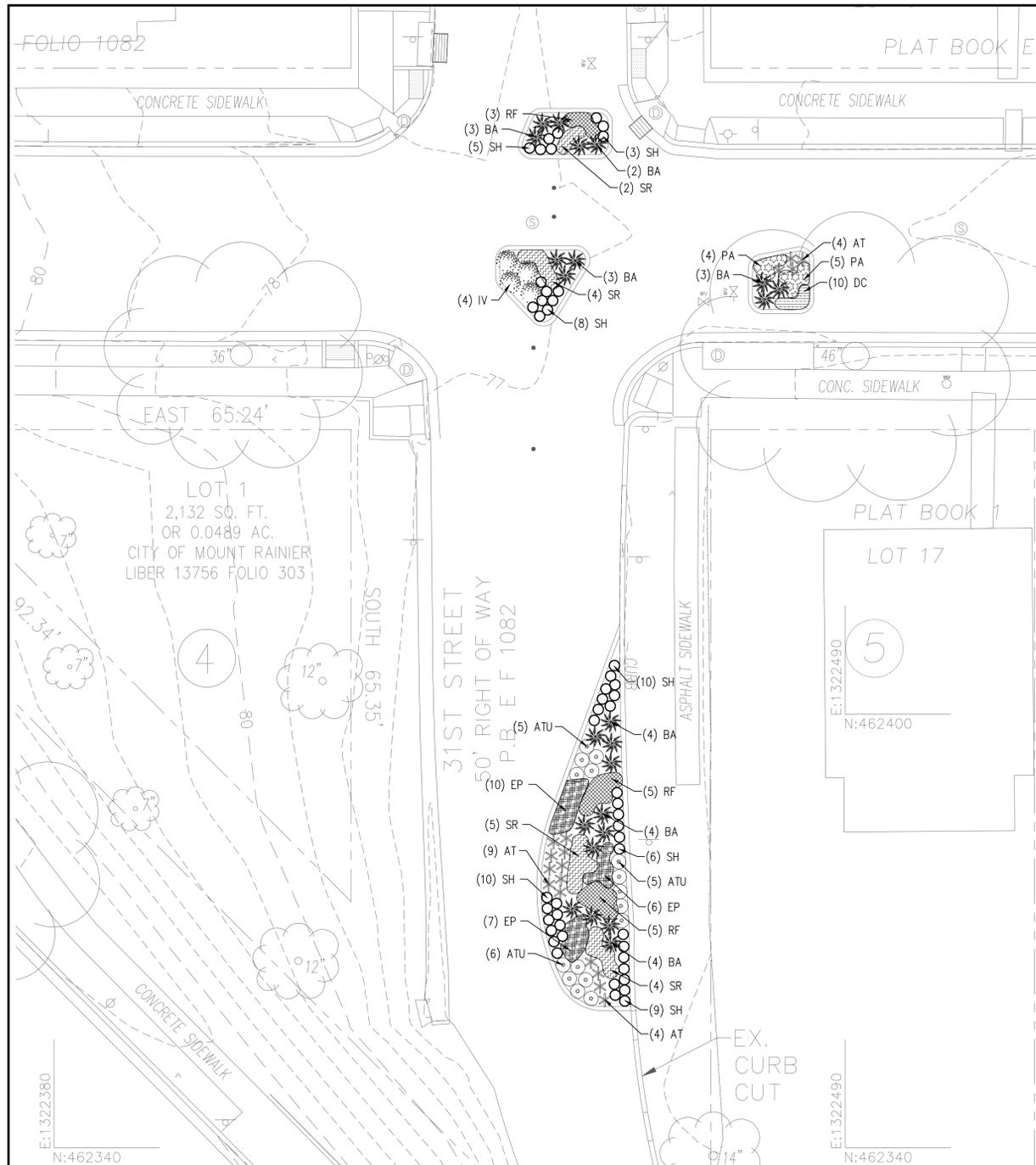
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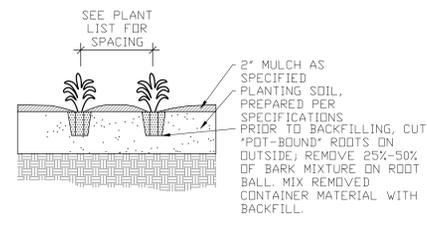
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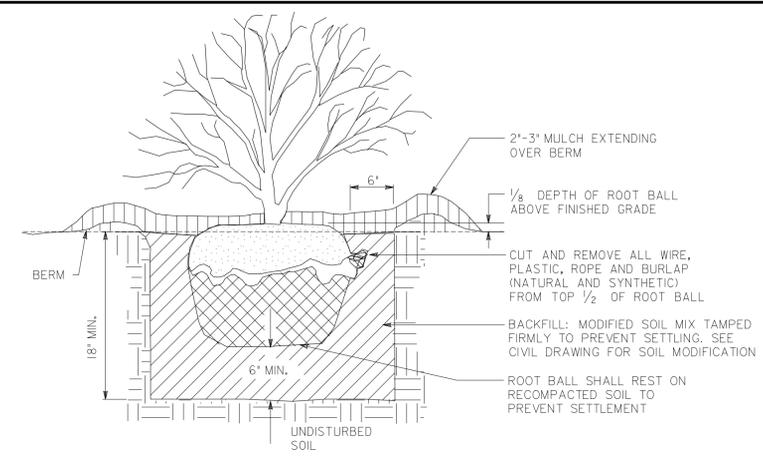
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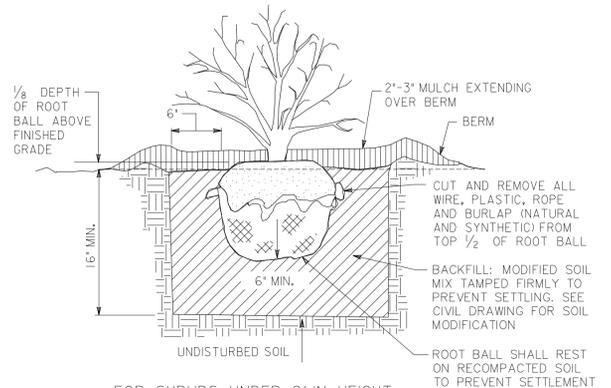
A TRIANGULAR SPACING PLAN NOT TO SCALE



B STANDARD PERENNIAL PLANTING SECTION NOT TO SCALE



FOR SHRUBS 2' TO 4' IN HEIGHT



FOR SHRUBS UNDER 2' IN HEIGHT

C STANDARD SHRUB PLANTING SECTION NOT TO SCALE

LANDSCAPE SPECIFICATIONS

A. PLANT MATERIALS
THE LANDSCAPE CONTRACTOR SHALL FURNISH AND INSTALL AND/OR DIG, BALL, BURLAP, AND TRANSPLANT ALL OF THE PLANT MATERIALS CALLED FOR ON THE DRAWINGS AND/OR LISTED IN THE PLANT SCHEDULE.

B. PLANT NAMES
PLANT NAMES USED IN THE PLANT SCHEDULE SHALL BE IDENTIFIED IN ACCORDANCE WITH HORTUS THIRD, BY L.H. BAILEY, 1976.

C. PLANT STANDARDS
ALL PLANT MATERIALS SHALL BE EQUAL TO OR BETTER THAN THE REQUIREMENTS OF THE "AMERICAN STANDARD FOR NURSERY STOCK," LATEST EDITION, AS PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN (HEREAFTER REFERRED TO AS AAN STANDARDS). ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES AND VARIETY, SHALL HAVE A NORMAL HABIT OF GROWTH, AND SHALL BE FIRST QUALITY, SOUND, VIGOROUS, WELL-BRANCHED AND WITH HEALTHY WELL-FURNISHED ROOT SYSTEMS. THEY SHALL BE FREE OF DISEASE, INSECT PESTS AND MECHANICAL INJURIES.

(1) ALL PLANTS SHALL BE NURSERY GROWN AND SHALL HAVE BEEN GROWN UNDER THE SAME CLIMATIC CONDITIONS AS THE LOCATION OF THIS PROJECT FOR AT LEAST TWO YEARS BEFORE PLANTING. NEITHER HEELED-IN PLANTS NOR PLANTS FROM COLD STORAGE WILL BE ACCEPTED.

(2) COLLECTED PLANTS OR TRANSPLANTED TREES MAY BE CALLED FOR BY THE LANDSCAPE ARCHITECT AND USED, PROVIDED, HOWEVER, THAT LOCATIONS AND SOIL CONDITIONS WILL PERMIT PROPER BALLING.

D. PLANT MEASUREMENTS
(1) MINIMUM SIZE FOR PLANTING SHRUBS SHALL BE, IN GENERAL, EIGHTEEN TO TWENTY-FOUR (18-24) INCHES IN HEIGHT OR SPREAD, AS APPROPRIATE, EXCEPT THAT A LARGER SIZE MAY BE REQUIRED WHEN DEEMED APPROPRIATE BY THE PLANNING DIRECTOR (OR DESIGNER) IN THE CASE OF PARTICULAR SPECIES OR PLANTING SITUATIONS.

E. PLANTING METHODS
ALL PROPOSED PLANT MATERIAL THAT MEETS THE SPECIFICATIONS IN SECTION B. ABOVE ARE TO BE PLANTED IN ACCORDANCE WITH THE FOLLOWING PLANTING METHODS DURING THE PROPER SEASONS AS DESCRIBED BELOW.

(1) **PLANTING SEASONS**
A PROFESSIONAL HORTICULTURALIST/NURSERYMAN SHALL BE CONSULTED TO DETERMINE THE PROPER TIME, BASED ON PLANT SPECIES AND WEATHER CONDITIONS, TO MOVE AND INSTALL PARTICULAR PLANT MATERIAL TO MINIMIZE STRESS TO THE PLANT. PLANTING OF DECIDUOUS MATERIAL MAY BE CONTINUED DURING THE WINTER MONTHS PROVIDED THERE IS NO FROST IN THE GROUND AND FROST-FREE TOP SOIL. PLANTING MIXTURES ARE USED. MONITOR WEATHER CONDITIONS AND AVOID PLANTING IF SOIL ON SITE IS TOO WET. LANDSCAPE PLUGS MUST BE INSTALLED WHILE THEY ARE IN ACTIVE GROWTH ONLY.

(2) **DIGGING**
ALL PLANT MATERIAL SHALL BE DUG, BALLED AND BURLAPPED (B+B) OR BARE ROOT IN ACCORDANCE WITH THE "AAN STANDARDS."

(3) **EXCAVATION OF PLANT PITS**
THE LANDSCAPE CONTRACTOR SHALL EXCAVATE ALL PLANT PITS, VINE PITS, HEDGE TRENCHES AND SHRUB BEDS AS FOLLOWS:

(A) ALL PITS SHALL BE GENERALLY CIRCULAR IN OUTLINE, WITH BOWL SHAPED SIDES. THE TREE PIT SHALL BE DEEP ENOUGH TO ALLOW ONE-EIGHTH (1/8) OF THE BALL TO BE ABOVE THE EXISTING GRADE. PLANTS SHALL REST ON UNDISTURBED EXISTING SOIL OR WELL COMPACTED BACKFILL. THE TREE PIT MUST BE A MINIMUM OF NINE (9) INCHES LARGER ON EVERY SIDE THAN THE BALL OF THE TREE.

(B) IF AREAS ARE DESIGNATED AS SHRUB BEDS OR HEDGE TRENCHES, THEY SHALL BE CULTIVATED TO AT LEAST EIGHTEEN (18) INCHES IN DEPTH MINIMUM. AREAS DESIGNATED FOR GROUND COVERS AND VINES SHALL BE CULTIVATED TO AT LEAST TWELVE (12) INCHES IN DEPTH MINIMUM.

(4) **PLANT PRUNING, EDGING, AND MULCHING**
(A) EACH TREE, SHRUB OR VINE SHALL BE PRUNED IN AN APPROPRIATE MANNER TO ITS PARTICULAR REQUIREMENTS, IN ACCORDANCE WITH ACCEPTED STANDARD PRACTICES AS STATED IN ANSI STANDARDS A300 FOR PRUNING. BROKEN OR BRUISED BRANCHES SHALL BE REMOVED WITH CLEAN CUTS MADE ON AN ANGLE FROM THE BARK RIDGE TO THE BRANCH COLLAR, NO FLUSH CUTS, TO MINIMIZE THE AREA CUT. ALL CUTS SHALL BE MADE WITH SHARP TOOLS. TRIM ALL EDGES SMOOTH. NO TREE WOUND DRESSINGS SHALL BE APPLIED.

(B) AFTER CULTIVATION, ALL PLANT MATERIALS SHALL BE MULCHED WITH A TWO TO THREE (2-3) INCH LAYER OF AGED SINGLE OR DOUBLE SHREDDED HARDWOOD MULCH OR CHIPS OVER THE ENTIRE AREA OF THE BED OR SAUCER. REFER TO THE MARYLAND STORMWATER MANAGEMENT DESIGN MANUAL.

(5) **SUBSTITUTIONS, REPLACEMENT, AND UNACCEPTABLE PLANTS**
(A) CONTRACTOR SHALL SUBMIT A WRITTEN MODIFICATION REQUEST TO INSTALL PLANTS OF DIFFERENT SPECIES, CULTIVARS, SIZES, GROWTH HABITS, OR PLANTING STOCK TYPE. SUBSTITUTIONS MUST BE APPROVED BY THE DESIGNER BEFORE THE PLANTS ARE BROUGHT TO THE SITE.

(B) PROMPTLY REMOVE AND REPLACE PLANTS THAT ARE UNACCEPTABLE AT ANY TIME DURING INSTALLATION AS SPECIFIED IN MD SHA 920.07, OR WHEN REQUESTED. PLANTS THAT ARE DETERMINED TO BE MISSING, DEAD, DYING, DAMAGED, DISEASED, DEFORMED, UNDERDEVELOPED, DAMAGED BY PESTICIDES, OR NOT TRUE TO SPECIES, CULTIVAR, SIZE OR QUALITY SHALL BE REPLACED.

F. SEEDING AND SODDING
ALL SEEDING AND SODDING SHALL BE AS PER 1994 STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL.

G. TOP SOIL
TOP SOIL SHALL BE RETAINED AND/OR PROVIDED ON ALL SITES AND SPREAD OVER ALL UNIMPROVED AREAS. REFER TO MD SHA SECTION 920.01.01.

H. MAINTENANCE
THE CONTRACTOR SHALL WATER, PRUNE, AND CULTIVATE TREES TWICE DURING THE FIRST TWENTY-FOUR (24) HOURS AND AS-NEEDED DURING THE FIRST GROWING SEASON. MAINTAIN PLANTS FOR 12 MONTHS AFTER INSTALLATION, UNTIL FINAL ACCEPTANCE.

I. WARRANTY
WARRANTY PERIOD FOR ALL PLANT MATERIAL SHALL BE FOR ONE YEAR. WARRANTY PERIOD BEGINS AT THE DATE OF FINAL ACCEPTANCE OF THE PROJECT.

ALL OTHER LANDSCAPE MATERIALS AND SPECIFICATIONS NOT NOTED ABOVE SHALL CONFORM TO MD SHA LANDSCAPE STANDARDS AND SPECIFICATIONS.

TYPE	QTY	SYMBOL	SCIENTIFIC NAME	COMMON NAME	SIZE	SPACING	REMARKS
SHRUBS	4	IV	ITEA VIRGINICA 'HENRY'S GARNET'	VIRGINIA SWEETSPIRE	3 GAL	36" O.C.	
PERENNIALS	17	AT	AMSONIA TABERNAEMONTANA	BLUESTAR	#SP4 (QUART) POT	24" O.C.	
	16	ATU	ASCLEPIAS TUBEROSA	BUTTERFLY WEED	#SP4 (QUART) POT	24" O.C.	
	23	BA	BAPTISIA AUSTRALIS	BLUE FALSE INDIGO	#SP4 (QUART) POT	36" O.C.	
	23	EP	ECHINACEA PURPUREA	PURPLE CONEFLOWER	#SP4 (QUART) POT	12" O.C.	
	9	PA	PACKERA AUREA	GOLDEN RAGWORT	#SP4 (QUART) POT	12" O.C.	
	13	RF	RUDBECKIA FULGIDA	BLACK EYED SUSAN	#SP4 (QUART) POT	12" O.C.	
	15	SR	SOLIDAGO RUGOSA 'FIREWORKS'	FIREWORKS GOLDENROD	1 GAL	24" O.C.	
GRASSES, FERNS, RUSHES	10	DC	DESCHAMPSIA CESPITOSA	TUFTED HAIR GRASS	#SP4 (QUART) POT	12" O.C.	
	50	SH	SPOROBOLUS HETEROLEPIS	PRAIRIE DROPSOED	3 GAL	18" O.C.	

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MISS UTILITY NOTE

INFORMATION CONCERNING EXISTING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS. THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATION OF ALL EXISTING UTILITIES AND UTILITY CROSSINGS BY DIGGING TEST PITS BY HAND, WELL IN ADVANCE OF THE START OF EXCAVATION. CONTACT "MISS UTILITY" AT 1-800-257-7777, 48 HOURS PRIOR TO THE START OF EXCAVATION. IF CLEARANCES ARE LESS THAN SHOWN ON THIS PLAN OR TWELVE (12) INCHES, WHICHEVER IS LESS, CONTACT THE ENGINEER AND THE UTILITY COMPANY BEFORE PROCEEDING WITH CONSTRUCTION. CLEARANCES LESS THAN NOTED MAY REQUIRE REVISIONS TO THIS PLAN.

REV. NO.	DATE	REVISIONS PRIOR TO APPROVAL

LANDSCAPE PLAN

31ST ST. AND PERRY ST. INTERSECTION

MOUNT RAINIER, MD 20712
DISTRICT 2
PRINCE GEORGE'S COUNTY, MARYLAND

ISSUE:	DATE: 08/26/2024
SCALE: 1"=10'	SHEET: 14 OF 14
FILE NO:	L-1.00
DRAFTED: GK	
CHECKED: DM	